

Concepts for improving streets walkways, and private development in the Clifton Fruitvale area.



Views of landmarks, such as Mt Garfield at left are important identity features for the Clifton / Fruitvale area

SKETCH CONCEPTS Ways to improve the community landscape.

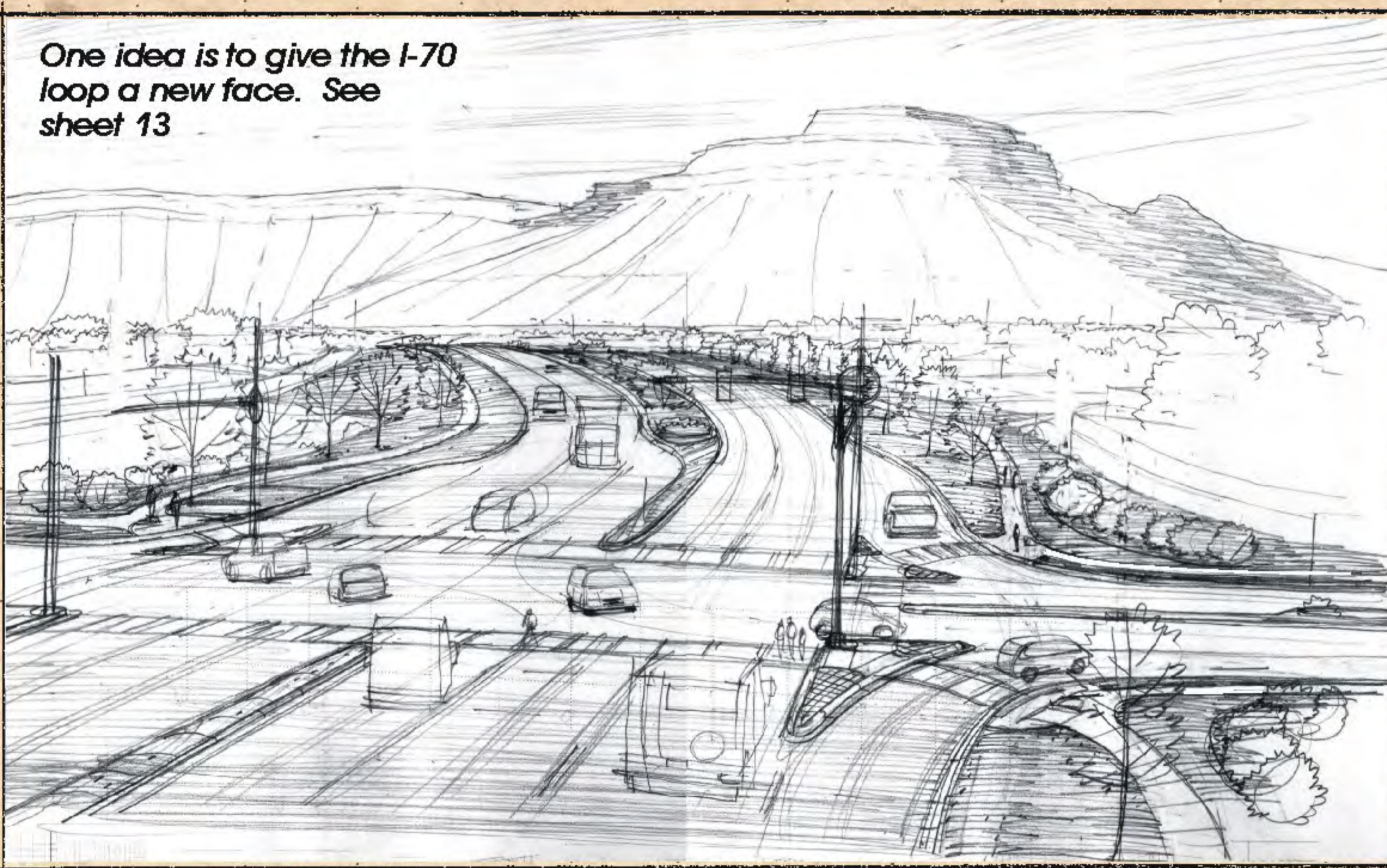
Sidewalks, walkways, and paths are part of the common-space fabric of a community. Improving how these function can go a long way to creating more livable, enjoyable neighborhoods. The ability to walk to school, to the store, or to a friend's home easily, unencumbered by hazards, should be a primary goal of any community. All centers of activity such as schools, churches, and shopping areas should be easily and safely accessed by nearby residents.

A corresponding goal is to enhance these community connections with the appropriate landscape elements such as street trees, landscape, and the appropriate fences or walls. Creating a pleasant, less harsh network of streetscapes can unite communities in a positive way. Private and public involvement can be coordinated to achieve these goals. Some of these goals can also be incorporated into bigger future projects and numerous smaller improvements throughout the Clifton / Fruitvale area.

Special areas such as gateways, important roadways, and historic areas can require special attention to maximize the benefits to the citizens. These in turn can be catalysts for energizing other improvements.

This sketch report identifies some basic guidelines to improve walkways, streetscapes, landmarks, fences and landscapes. It is hoped that these will aid in public discussion and awareness.

** Friendly walkways, entryways, and landscaped streets are important community pride and identity elements.*

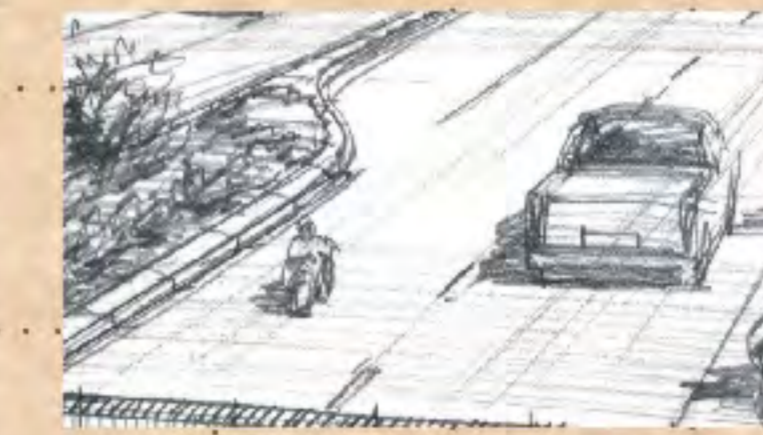


One idea is to give the I-70 loop a new face. See sheet 13

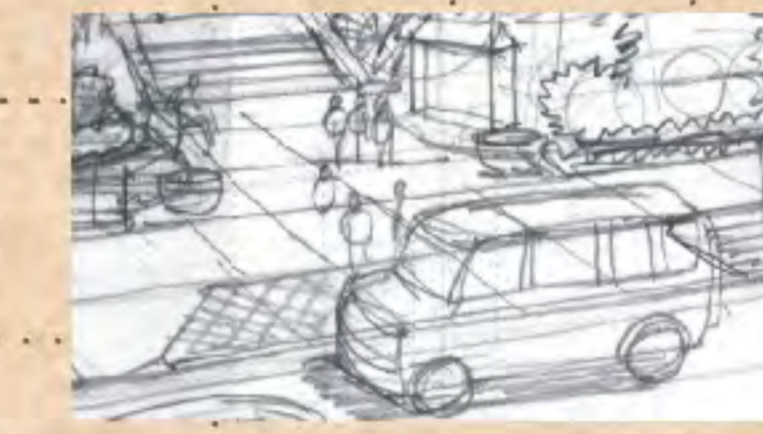
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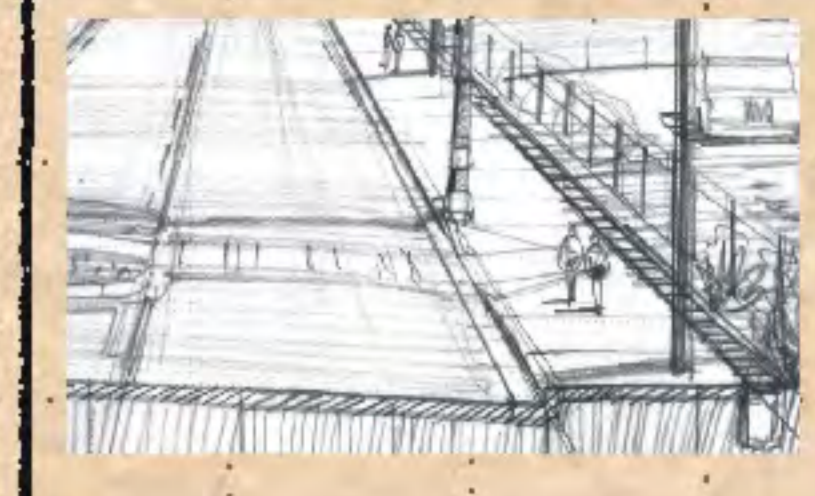


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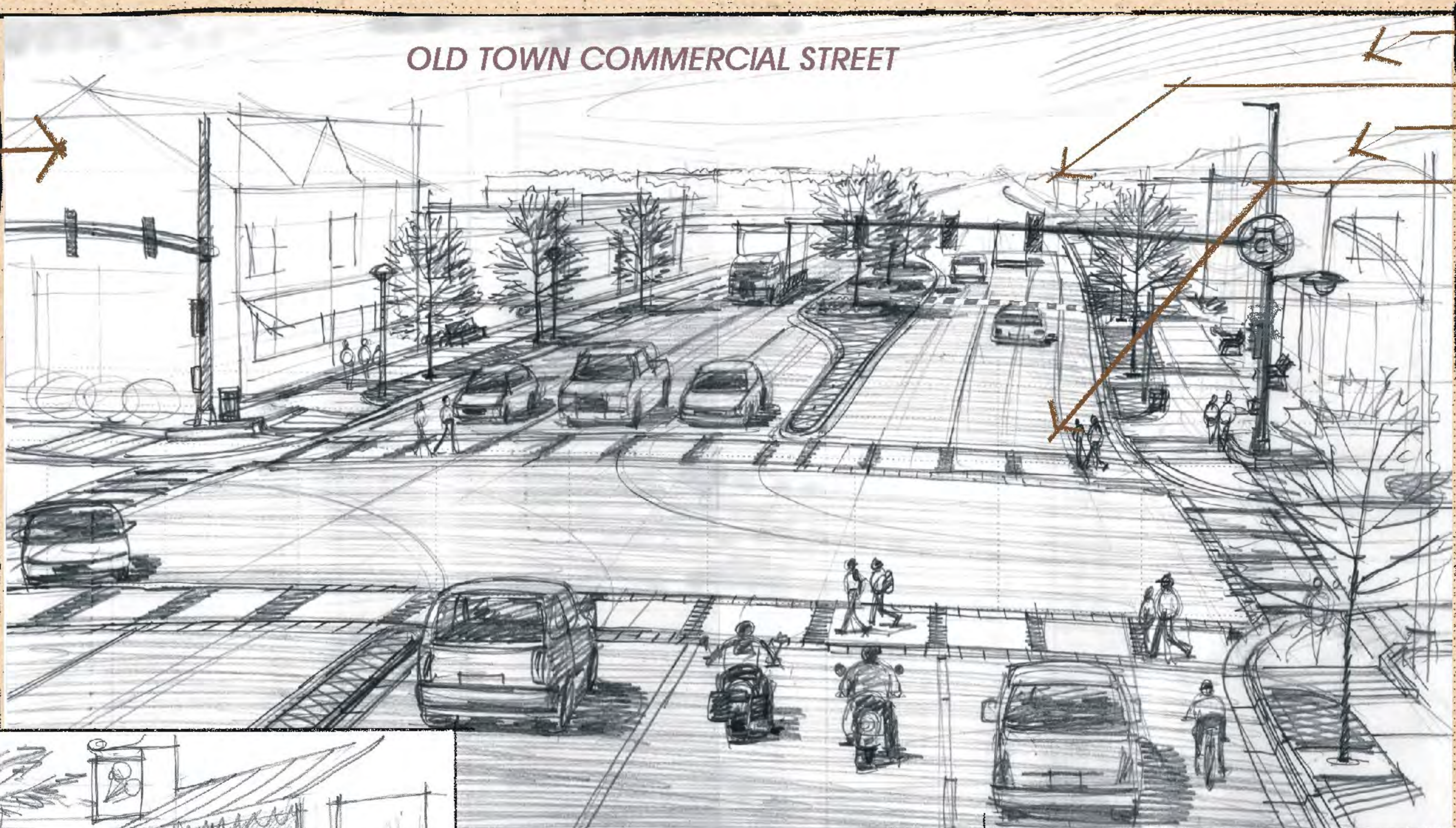
sheet 13
I-70 Loop



Sheet 14
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sheet 15
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TOP CENTER: Bird's eye view of older style commercial area, or community node, with modest streetscape upgrades. Specialty pavements and street trees are added to roadsides to enhance these important neighborhoods.

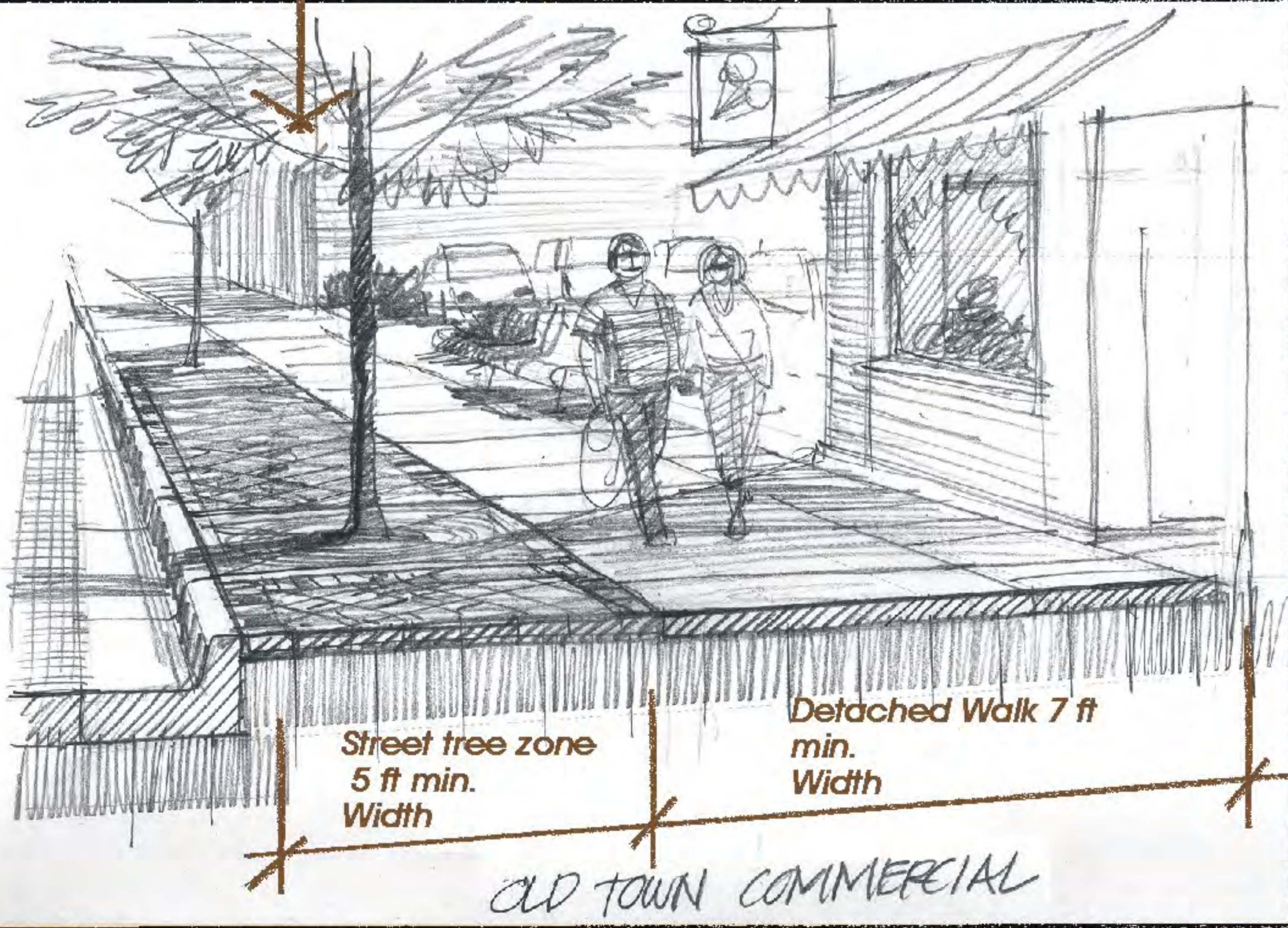
Cross walks are also emphasized for pedestrian safety and as a visual cue to motorists.

Median landscape may be appropriate in some areas.

BELOW: Sidewalk cross section showing specialty paving in the tree lawn area and a widened sidewalk to accommodate larger groups of people and activities.

Business close to arterial street with parking in the rear.
 Median landscape where appropriate.
 Signal pole with neighborhood sign plaque.
 Enhanced cross walk, either a specialty pavement type or embossed asphalt

WALKWAYS SIDEWALKS, Old town commercial areas
 Older more historic areas or commercial areas that are close up to the arterial streets require a more refined walkway and streetscape treatment. Increased pedestrian traffic often means a wider side walk is desirable. Sketch at left indicates a 12 ft wide walkway area with a special paving area for what would be the tree lawn. This wider pavement accommodates larger groups of people, some street furnishings, light poles and perhaps some benches. Trees can be in 'tree grate' pit or in a small unpaved area. A more utilitarian approach would be to pave the entire wide walkway in concrete with openings for trees. This style of commercial development is being revived in many areas.

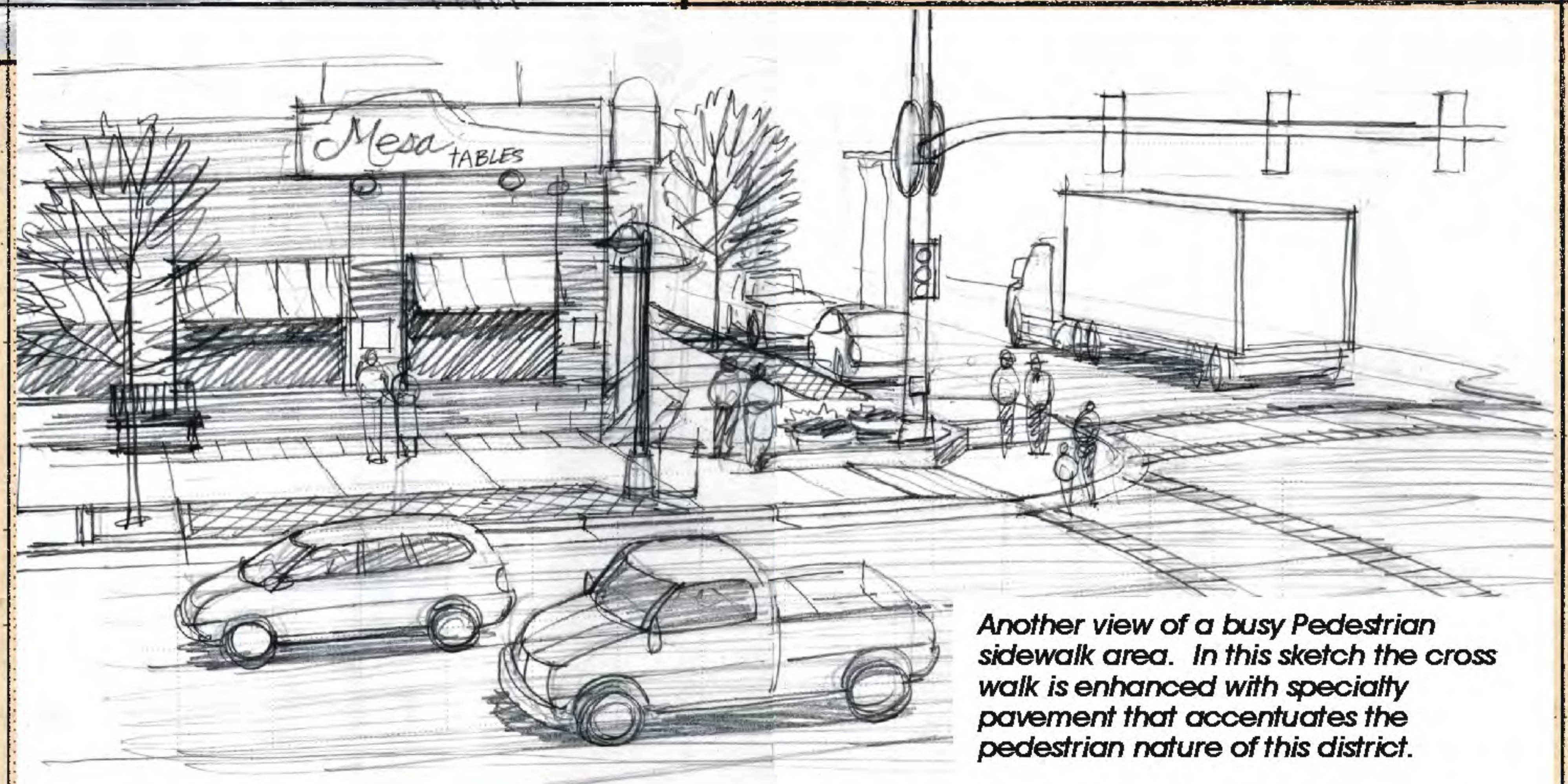


Street tree zone 5 ft min. Width
 Detached Walk 7 ft min. Width

OLD TOWN COMMERCIAL

ARCHITECTURAL CHARACTER of newer buildings and renovated buildings should build on the existing good architecture examples in the community, preferably the more historic structures. Projects should use similar materials in similar ways although with more contemporary function. Develop a vernacular of materials: windows, roof lines, and details that works with the climate, the economy, and the people.

** Busy community nodes deserve more extensive streetscape refinements*



Another view of a busy Pedestrian sidewalk area. In this sketch the cross walk is enhanced with specialty pavement that accentuates the pedestrian nature of this district.

Walkways, nodes, commercial areas

BOULEVARDS

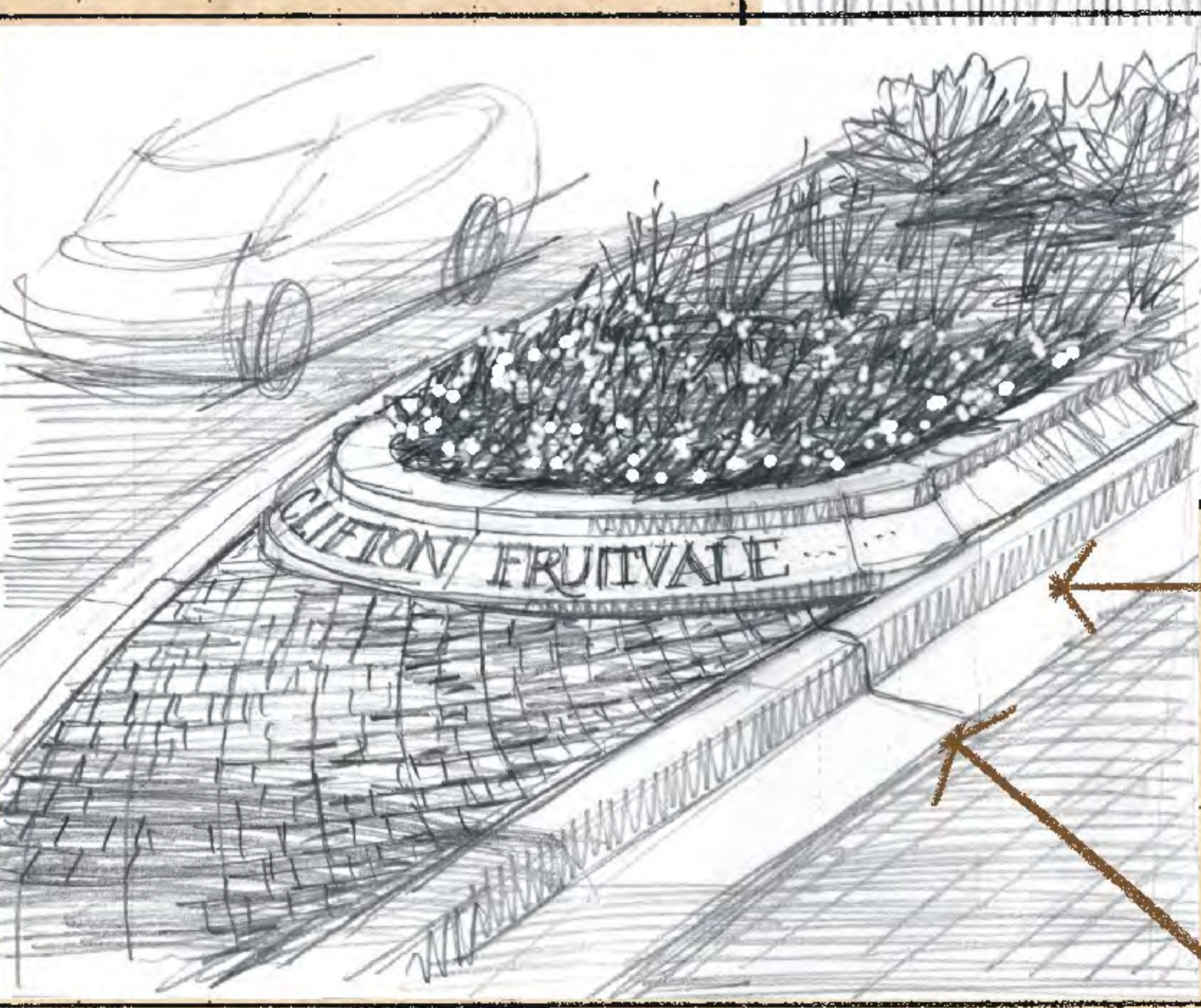
Arterial road, entry roadway, aligns with landmarks, connects important parts of town.

Boulevards are important roadways that are intended to enhance property values and generate community pride. As gateways, or entry ways they give visitors positive first impressions of a community. Locals also have an attractive pedestrian friendly corridor that connects neighborhoods and districts. Boulevards are landscaped enhanced arterial roadways. They can be commercial, residential or a combination of land uses. The landscaped medians are the unique feature. Only the wider medians can be landscaped. Narrower medians at left turn lanes are usually paved with specialty pavements.

* *Friendly walkways and landscaped streets are important community pride and identity elements.*

Views of landmarks, such as Mt Garfield, and Book Cliffs add to the experience

5ft wide Detached walkways
5 ft wide tree lawn
4 ft wide bike lane

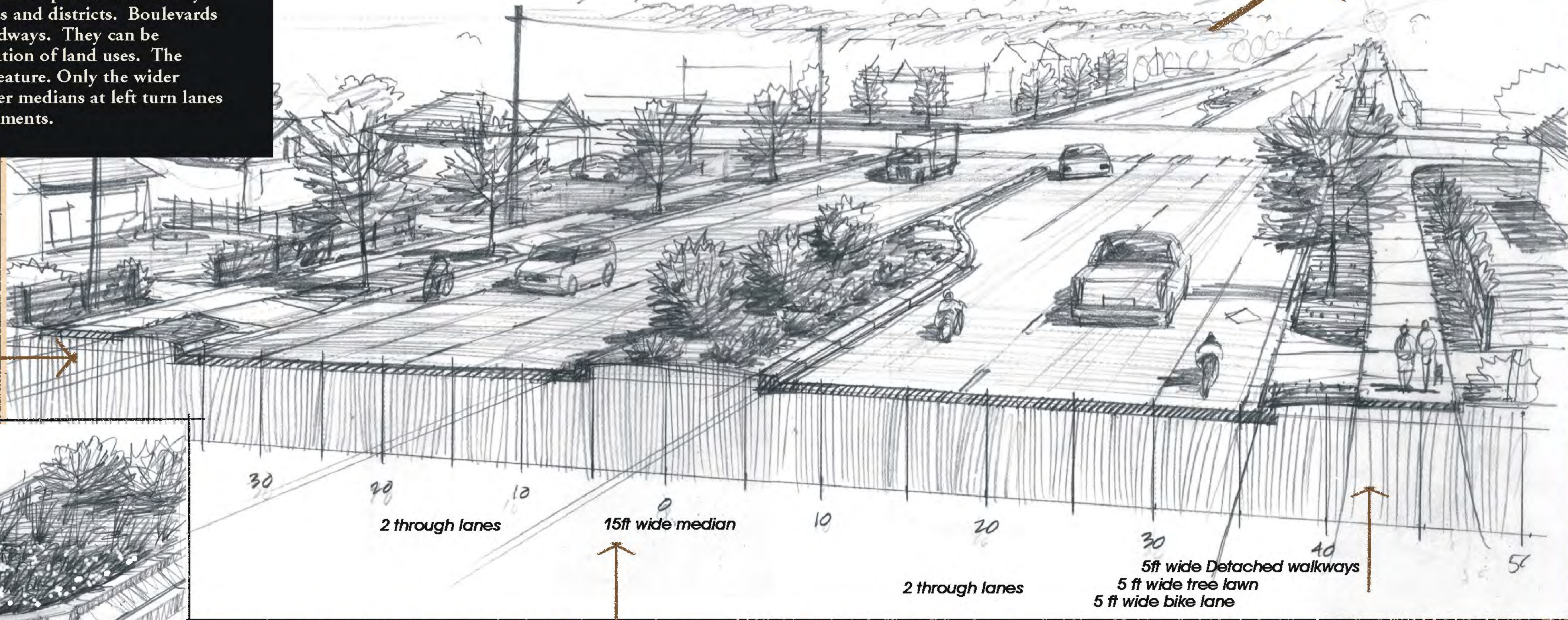


LEFT: Landscaped medians should be at least 8-10 ft wide to fit trees shrubs and some kind of median edging. Narrower medians are usually paved. Concrete, patterned concrete are the preferred materials.

Ends of medians can be highlighted with community names

ABOVE: Cross section of a landscaped boulevard. A 100 ft wide right of way width for a 5 lane roadway is typically enough width. This is an attractive and safe way to accommodate pedestrians, cyclists, and motorist.

Right: Example of attractive median landscape (Unaweep Ave.) in Grand Junction. Median edging is inspired by the Book Cliffs



Boulevards, landscaped arterials

Parking area, with landscape screening

Generous pedestrian provisions detached walks if possible. Street tree planting. If an historic site, period paving or site furnishings may be appropriate.

Well designed and maintained landscapes are important



LANDMARK WITHIN THE COMMUNITY

Landmarks may benefit from additional public open space to accommodate bigger groups of people or features to supplement landmark activities. Other considerations:

- *Interpretive signing and information kiosk
- *Public gathering area
- *Adequate drop off area for small busses.
- *Accessibility is also important, handrails, ramps are important elements.

LANDMARKS, HISTORIC SITES, Within a community.
 There are many kinds of landmark or historic structures. When they are located within the fabric of a developed community they may deserve some of the treatment shown at left. Many of the edge, walkway suggestions described in sheets 2 and 3 may apply here. Additional upgrades to these features may incorporate historic character materials or features such as brick, wrought iron fences, period lighting, or benches. Depending on how the site is used it may also need some public accommodation type features like parking lots, interpretive signing, drop off areas or as shown here, additional park space.

Landmarks

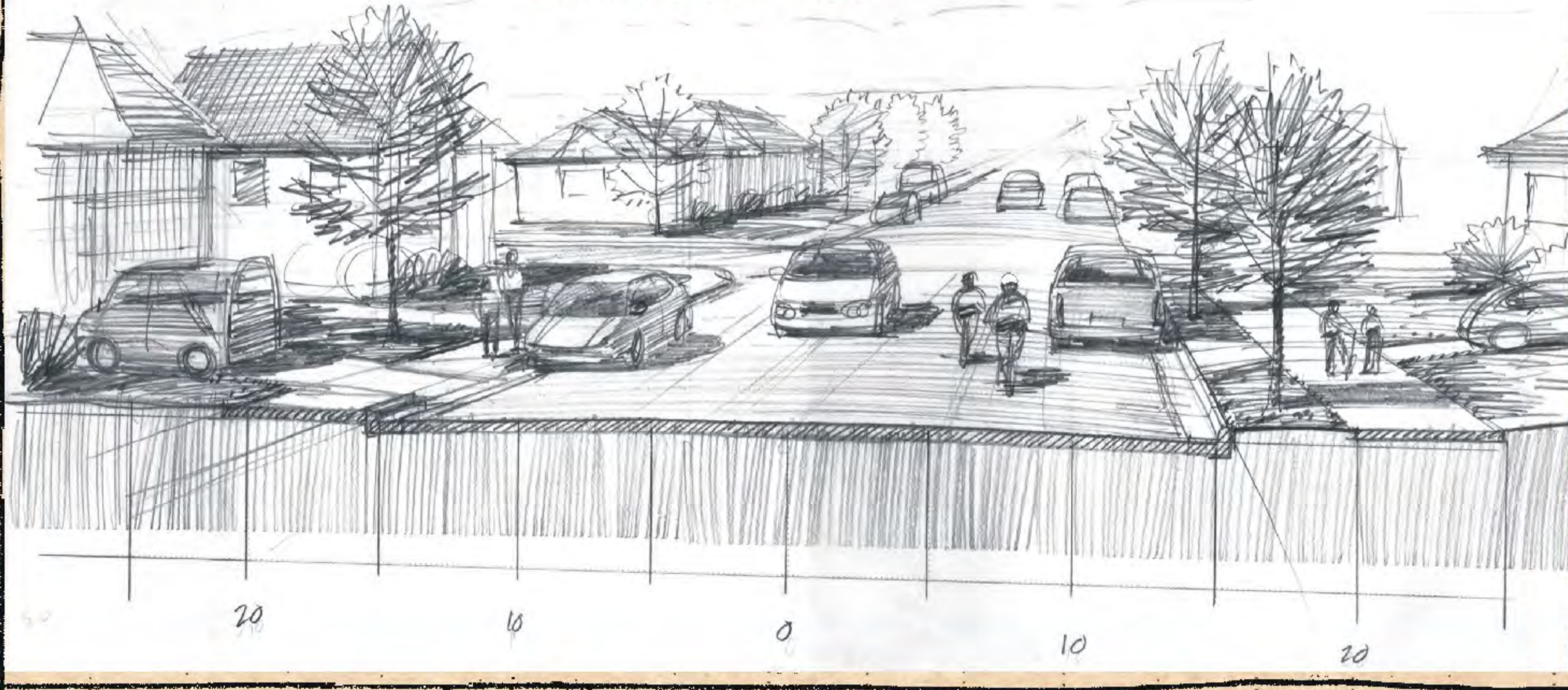
HISTORIC STRUCTURES or historic sites can encompass a wide variety of arrangements from true authentic restorations to adaptive re-uses. Historic 'period' restorations may deserve public interpretive facilities similar to that shown above. Private commercial partial restorations however may not. Many of these streetscape elements may, none-the-less be important in accentuating an important community landmark feature.

RIGHT: Other landmarks include major topographic or landscape features such as Mt Garfield. This is the major identity feature of the Clifton / Fruitvale area. An attractive street scape combined with the dramatic view of this mountain can make a lasting impression on visitors.



* Landmarks are key identity elements in communities. Enhancing the landmarks can do much for enhancing communities.

LOCAL RESIDENTIAL STREET WITH WALKWAYS



LEFT: Cross section of a local residential street with 27 ft road. A detached walk is shown on the right, an attached walk on the left. On-street parking is possible. Occasional two directional traffic with larger vehicles is slowed because of the tight fit. Which is appropriate for a residential area.

WALKWAYS SIDEWALKS
Low traffic Residential Streets

Pedestrian ways are one of the most important ingredients in livable successful communities. The ability to safely walk to school, walk to your neighbors house, the park, or to the store are fundamental needs. We shouldn't be compelled to drive everywhere or if we do walk, have to negotiate dangerous conditions. Indeed many people are unable to drive or do not have someone to drive them to their destinations. Walkways need to be safe and convenient.

Detached walkways, where the walkway is separated from the street by a tree lawn, is the preferred walkway type when adequate space is available. Attached walks, where the sidewalk is attached to the street curb, is acceptable but somewhat less desirable.

It can also be stated that walkways and sidewalks are aesthetically pleasing. When you drive through a community with generous walkways and street trees it has a welcoming friendly appearance. Neighborhoods without walkways often appear to be unfriendly and isolated.

Walkways, paths, trails

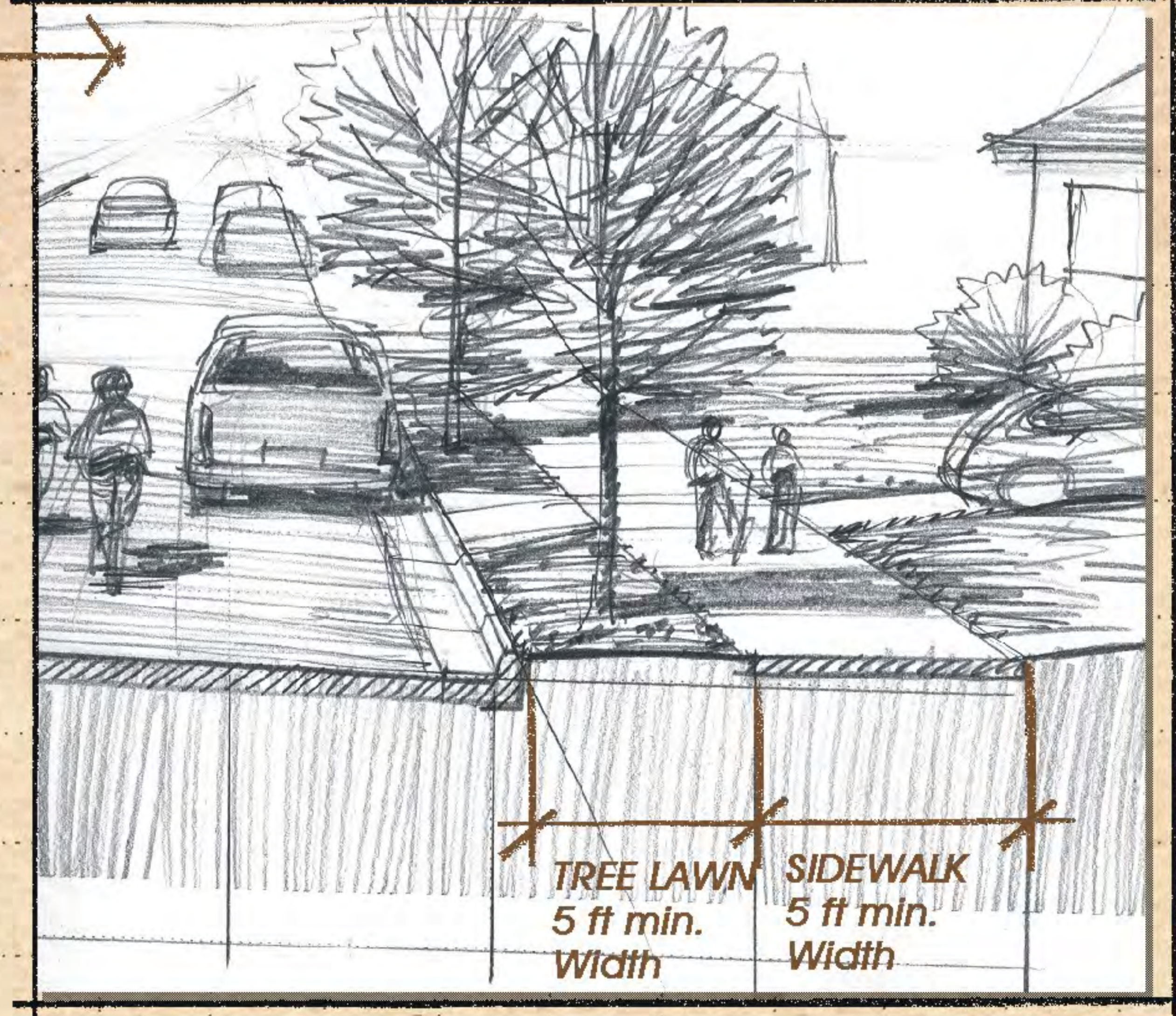
LEFT: Close-up of the attached walk. Attached walks need to be at least 7 ft wide. All walkways should allow two people to walk comfortably side by side. A five foot wide walk will allow this but with a curbed edge people shy away from the edge. Indeed the curb is also a hazard for small children on bikes, and older people with walkers and wheel chairs. A 7 ft wide attached walk minimizes these hazards and user conflicts.

RIGHT: Close-up of the detached walk. This has a classic home town look. The tree lawn space between the curb and the walk gives the pedestrian a safer feeling and creates a space for tree planting. Detached walks can be five ft wide and function pretty well. The tree lawn should be a minimum of 5 ft wide to allow for tree planting space.



ATTACHED WALKWAYS

* Minor residential streets should have walkways, preferably detached walkways with street trees.



DETACHED WALKWAYS

PART 1 SKETCH PORTFOLIO - Walkways along a minor arterial and collector roadways

Businesses need visibility and access. Tree planting should be more widely spaced. Shrub varieties should be 3-4 ft height allow views of building signs

Tree planting is often possible under utility poles using lower growing species.

Attached walkways should be a minimum of 7 ft width. Busier arterials may consider 8 ft or wider attached walks to better accommodate recreation cyclists.

Bike lanes, 4 ft minimum width, striped on pavement are desirable, particularly on regional bike trail links, or appropriate corridors. 5 ft is preferred width.

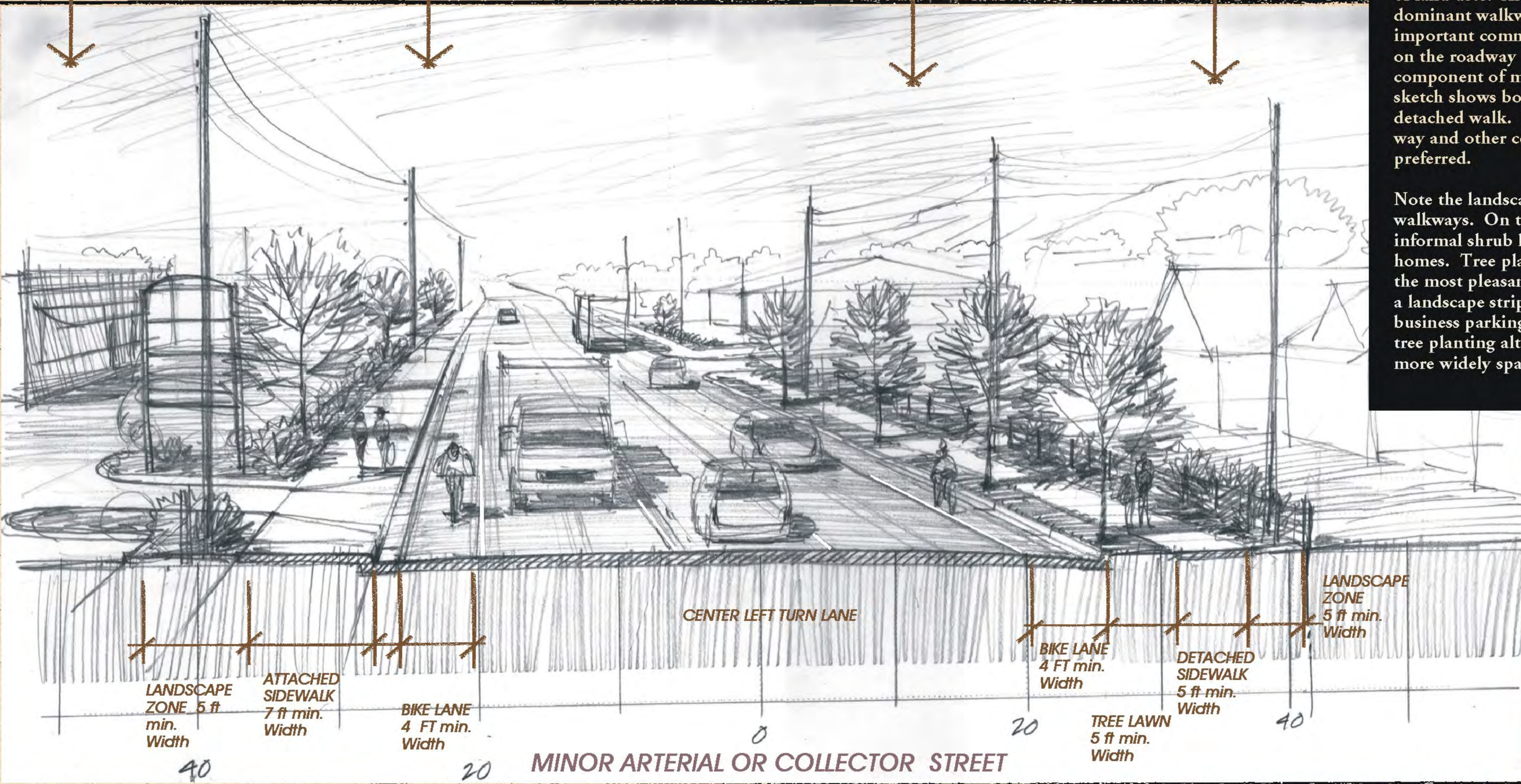
Detached walkways are more user friendly and should be provided wherever possible. The same minimum 5 foot wide tree lawn and 5 foot wide walkway apply here. If bike lanes are not provided on the street detached walkways should be widened to 8 ft where possible.

Fences and screening, see 'Edges' section of this report

WALKWAYS SIDEWALKS, BIKELANES Minor Arterial and Collector Streets

Minor arterial and collector streets like the one shown on the sketch often pass through a variety of land uses. Although vehicular traffic is more dominant walkways are still needed for important community connections. Bike lanes on the roadway are also an important component of multi modal transportation. This sketch shows both a wide attached walk and a detached walk. Depending on available right of way and other constraints either option may be preferred.

Note the landscape features associated with the walkways. On the right are low open fences and informal shrub hedges that screen back yards of homes. Tree planting on the tree lawn creates the most pleasant walk environment. On the left a landscape strip between the walkway and the business parking lot is used to screen cars. Street tree planting although still desirable may be more widely spaced to allow views of businesses.

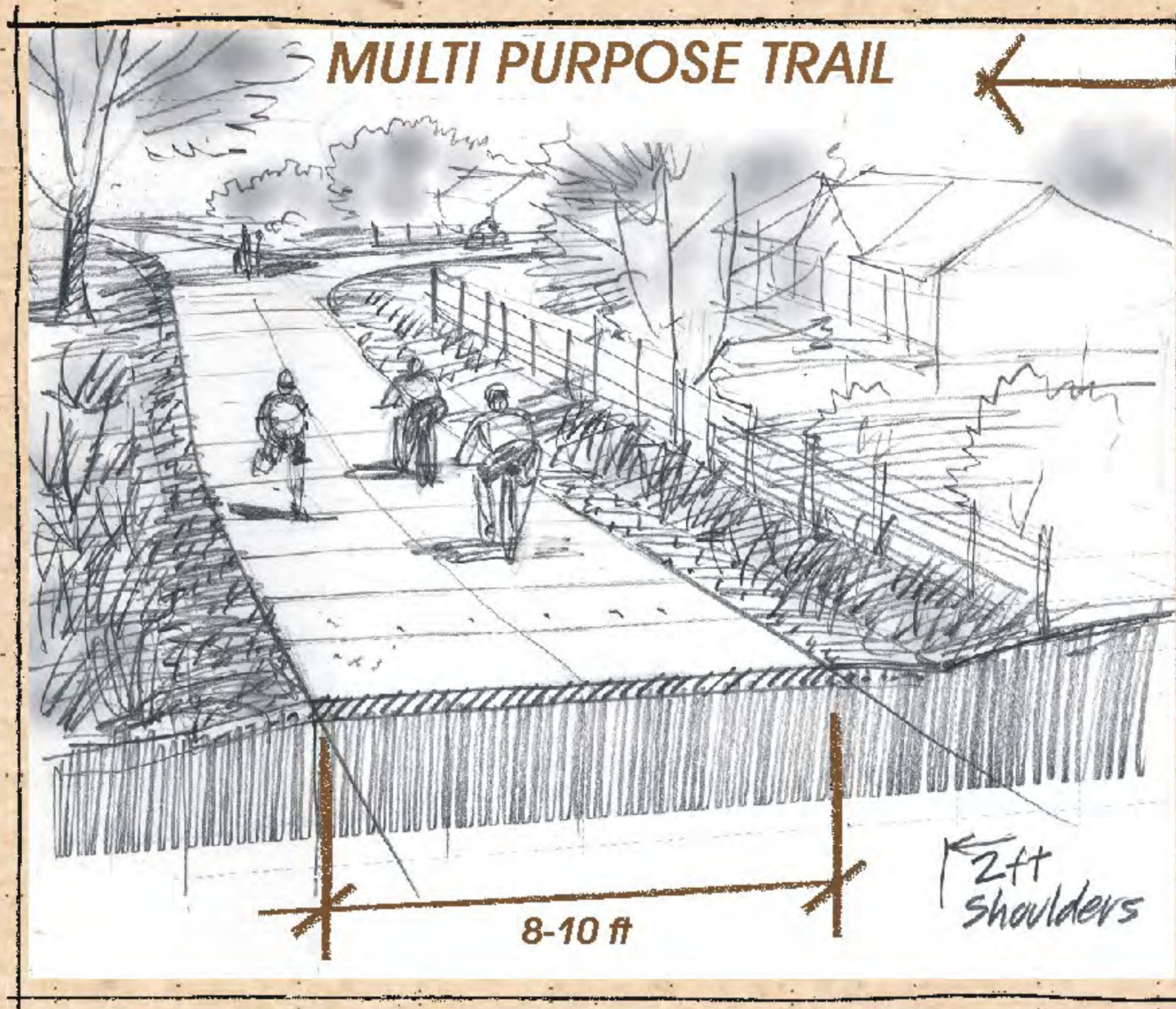


Walkways, along minor arterial roadway paths,

- * Most urban streets should have side walks..
- * Detached walks are the preferred type of walk.



Example of multi purpose trail paralleling a limited access highway. This is another way of providing pedestrian and bicycle connections for communities and neighborhoods.



MULTI PURPOSE TRAIL

LEFT: Multi purpose trail. A paved 8-10 foot wide trail with wide soft shoulders for horses and runners is preferred. Crushed fine textured gravel surface is also a good choice but requires more maintenance. Open fences at edges may be appropriate when passing through residential areas.

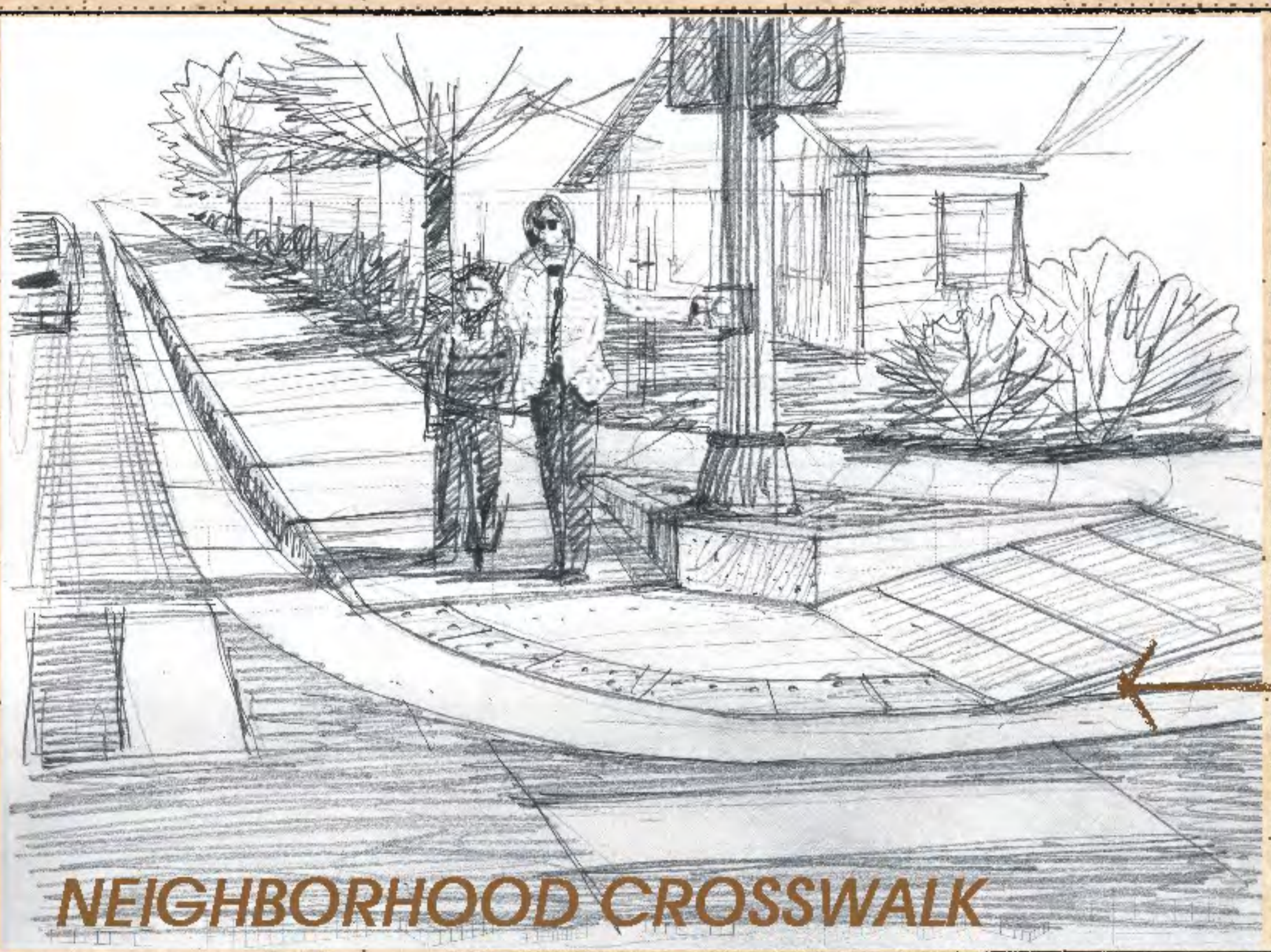
CROSS WALKS, TRAILS, AND PATHS

Residential areas, open space

Connecting neighborhoods to schools, parks, businesses, stores and other trails not only depends on good walkways but also on important community links like safe crosswalks. A safe cross walk can make all the difference in the world for a 10 year old being able to walk to school instead of being driven.

A short cut path at the end of a cul de sac or through a neighborhood can also be very helpful in connecting neighborhoods and businesses.

Multi purpose trails are both functional and recreational features. Good design of these features is important so they are safe and enjoyable.



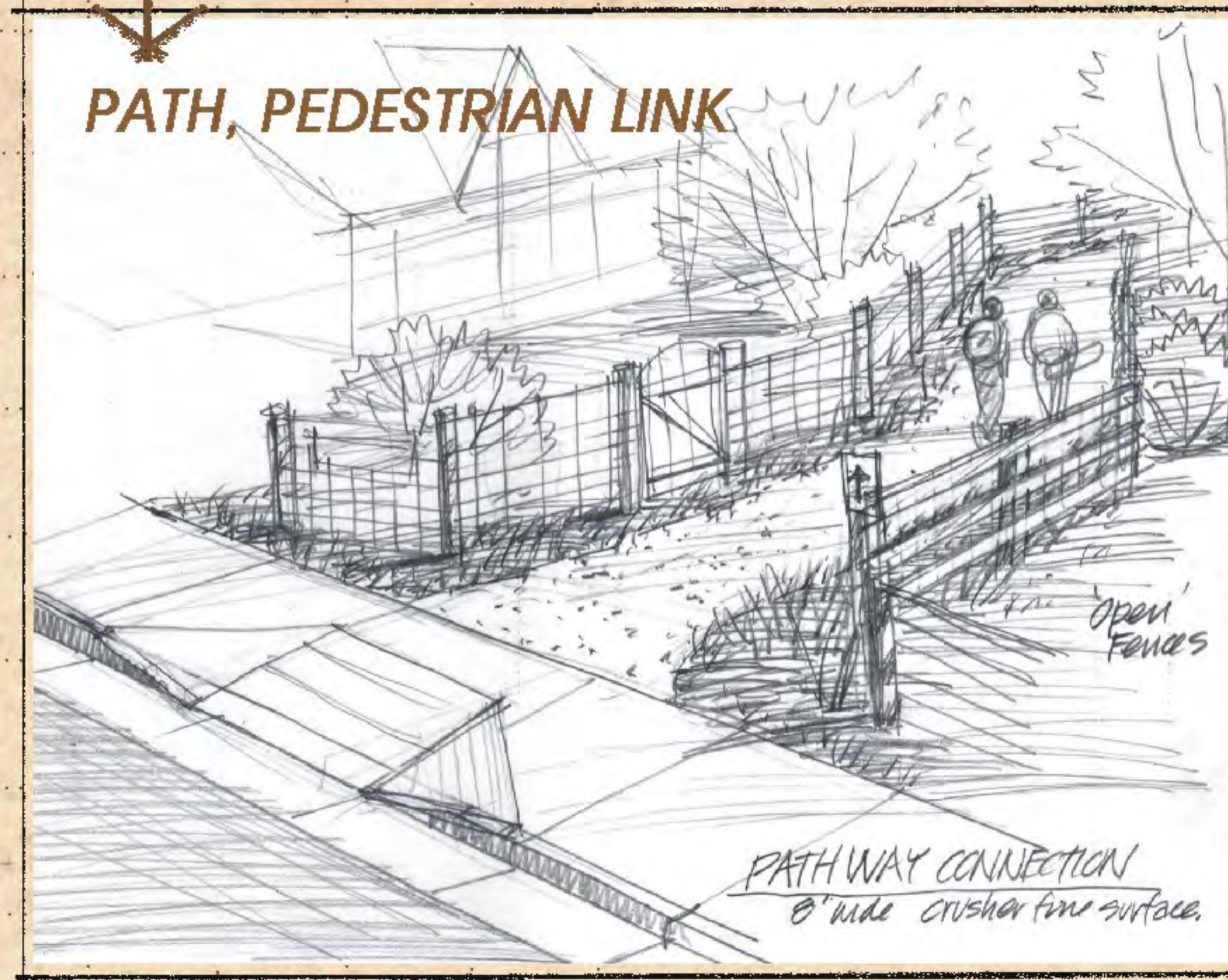
NEIGHBORHOOD CROSSWALK

Pedestrian ramp at cross walk. This sketch depicts more recent national accessibility standards. All ramps are a minimum 5 ft width. Low wall is intended as a tactile guide for visually impaired people. Note diagonal walk behind signal pole. In this sketch the cross walk is painted. More elaborate cross walk options might be appropriate at major pedestrian nodes. Appropriate signing is also desirable to alert drivers.



ABOVE: Illustration depicting a safer at-grade railroad crossing. Paved walkways, signals, and signs are important components. Although grade-separated crossings are safer they are not always possible.

* Critical links like crosswalks, ramps, and pathways are important components of walkway networks



PATH, PEDESTRIAN LINK

LOWER RIGHT: Connector path passing through a residential area. Mostly for pedestrians, these need to be only about 8 ft wide, between fences. Path users are less intimidated if the edge fences are 'open' type fences rather than solid. Wire mesh can be use on fences to keep dogs away. Fence shouldn't exceed 5 ft in height. The surface can be fine textured gravel compacted to create a smooth surface.

Walkways, paths, pedestrian ramps, trails





White vinyl fences are a popular choice presumably for their durability and cost. While they are better than some fence types they have a rather harsh bright shiny synthetic look. The open vinyl fence above could use some larger shrubs planted along the fence line to soften its look.



Another solid vinyl fence about 4 ft in height. This one is in shadow. In the bright sun the shiny reflective surface can be rather harsh. Note landscape space in front of fence. Large shrubs should be planted here to soften the view of the fence texture and enhance roadway.



This is a brick masonry wall about 4 1/2 ft tall at the top of a berm separating a parking lot from an arterial roadway. If a fence or wall is desirable this is a high quality attractive solution. Higher masonry walls, whether brick or block should have adequate landscape space in front of the wall for shrub planting.



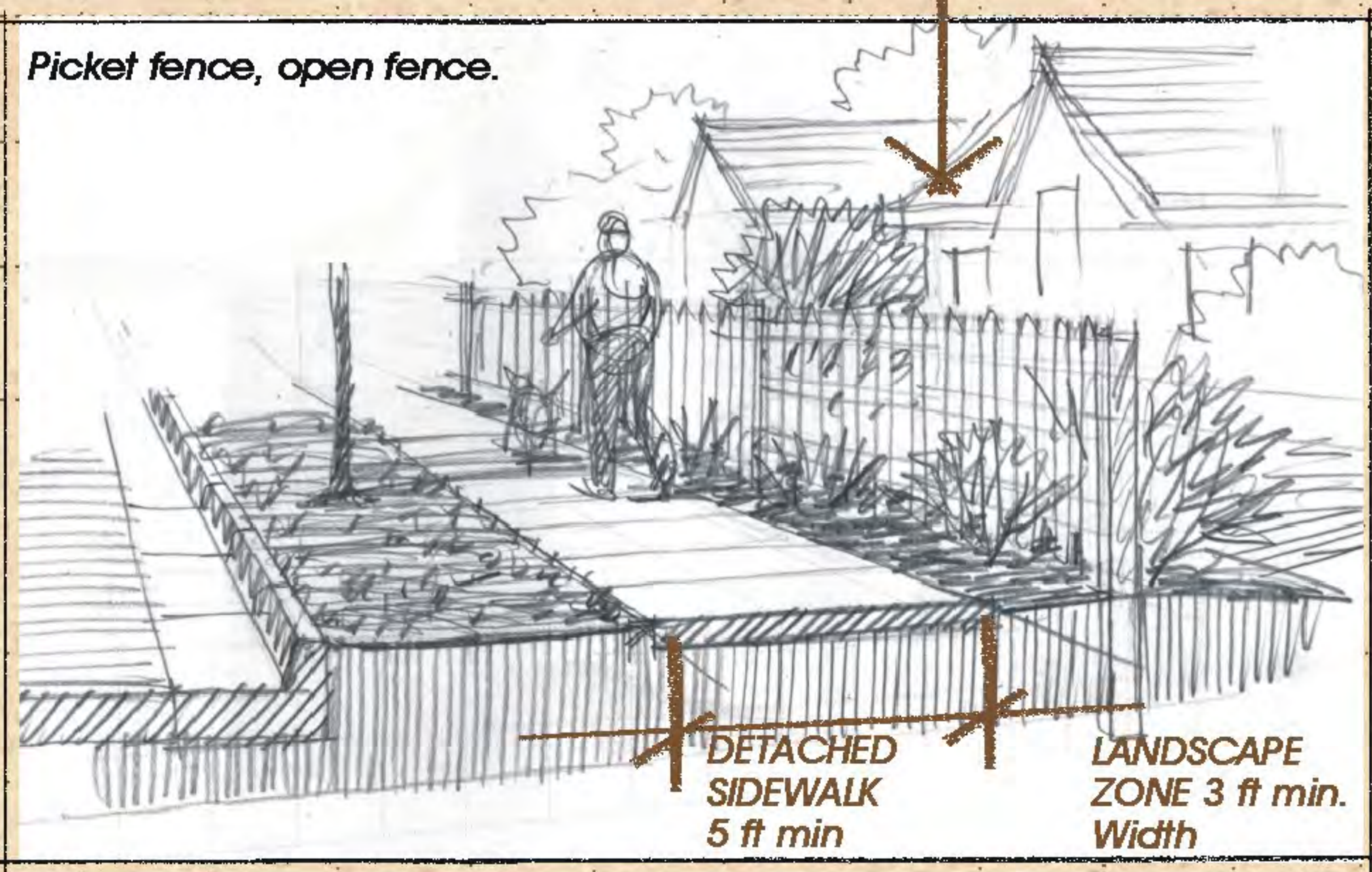
6 ft tall solid cedar fences are popular inexpensive options that have been overused in the past 30 years. Unfortunately these fences only last about 15-20 years. Along this arterial roadway the deteriorating fences become a neighborhood landscape blight issue. These are all private fences, some may get replaced others may not. What is most unfortunate is that there is no landscape space directly in front of the fence to soften the view of the tattered fences.

EDGES, FENCES
Along roadway edges
 Fences are used to create separations, to block views and define spaces. When placed along street frontages they often become the dominant landscape element. Most fences are built by property owners to define their property line and to screen the view of the roadway. Sometimes the roadway forms the edge of their back yard sometimes the front yard. Unfortunately a lot of these fences are stark and obtrusive, or just ugly. Often they are erected with out regard to the long term streetscape views. From the roadway side 6 ft tall solid fences convey a image of isolation and rejecting of the public roadway. More often than not there are no guidelines for fences that take the roadway landscape and views into consideration. It is not only the fence type but also where the fence is located in relation to walkways and the landscape spaces that can make a difference. Guidelines should encourage landscape spaces that separate the walkways from the fence or wall. Guideline should also discourage the use of fences taller than 5 ft for most frontages.

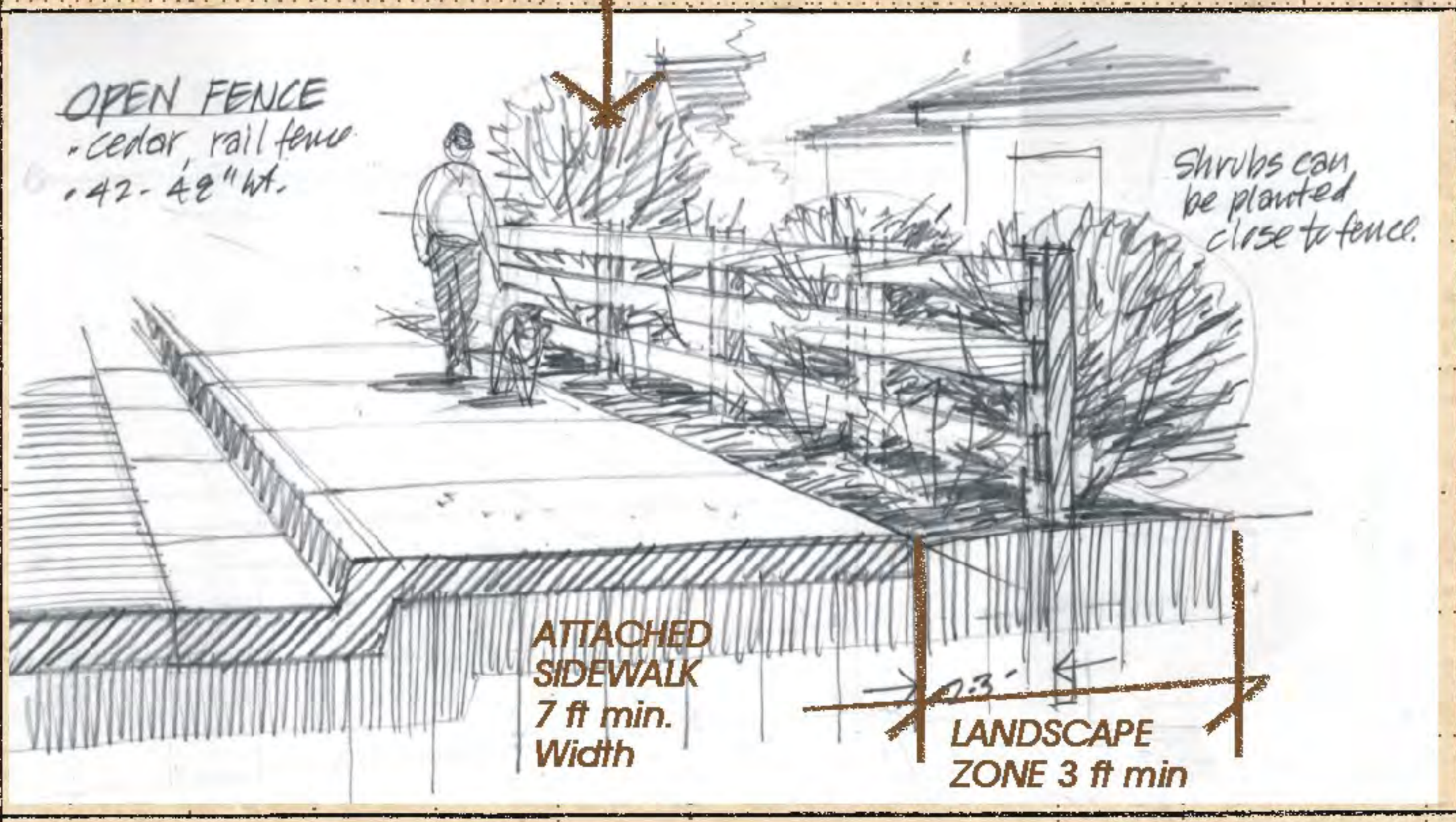
Edges, fences

PREFERRED FENCE ALTERNATIVES

BELOW: Sketch of picket fence alternative. Open fences, fences that allow some screened views, are often the most desirable when considering views from the roadway. Shrub planting along the fences can create a soft and welcome appearance.



BELOW: Sketch of open rail fence alternative. Both of these 3-5 ft height open fences create softer more attractive roadway edges. A variety of shrubs augment the screening ability of the fence and create a much more interesting and attractive roadway. Note with this rail fence that shrubs can be planted almost under the fence and grow through the openings. As the wood fence ages the older larger shrubs will conceal much of the fence wood.

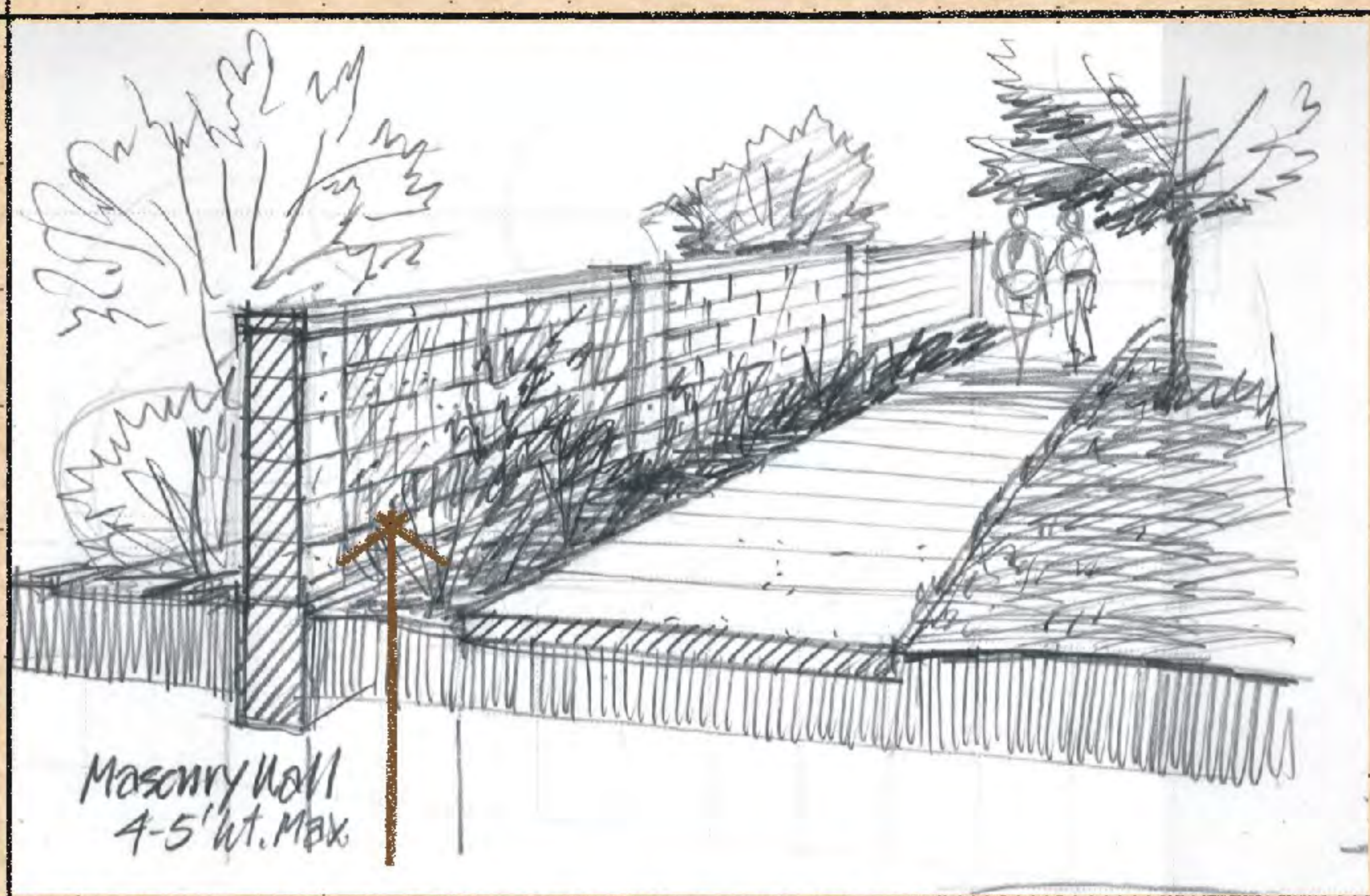


PRIVACY AND SAFETY ARE not a cut and dry issue. Privacy needs to be balanced with important visibility issues. Shutting all views to and from back yards does not help build community cohesiveness. Sometimes a more subtle separation is better than a solid fence or wall. There is even some evidence that solid fences inhibit crime awareness. Being able to see over a fence or through an open fence can have important advantages other than just aesthetics.

NOISE ABATEMENT-The solid wood fence above does little to attenuate noise from the roadway. While it blocks the views of the road, sound leaks through the cracks with little resistance. Nor does dense vegetation attenuate much sound. The most effective noise barriers are tall solid masonry walls or earthen berms such as the one shown above with the brick wall.

- * Open fences are usually better than solid fences;
- * Fences are best if they have a landscape zone in front of them.
- * Environmental design for public safety





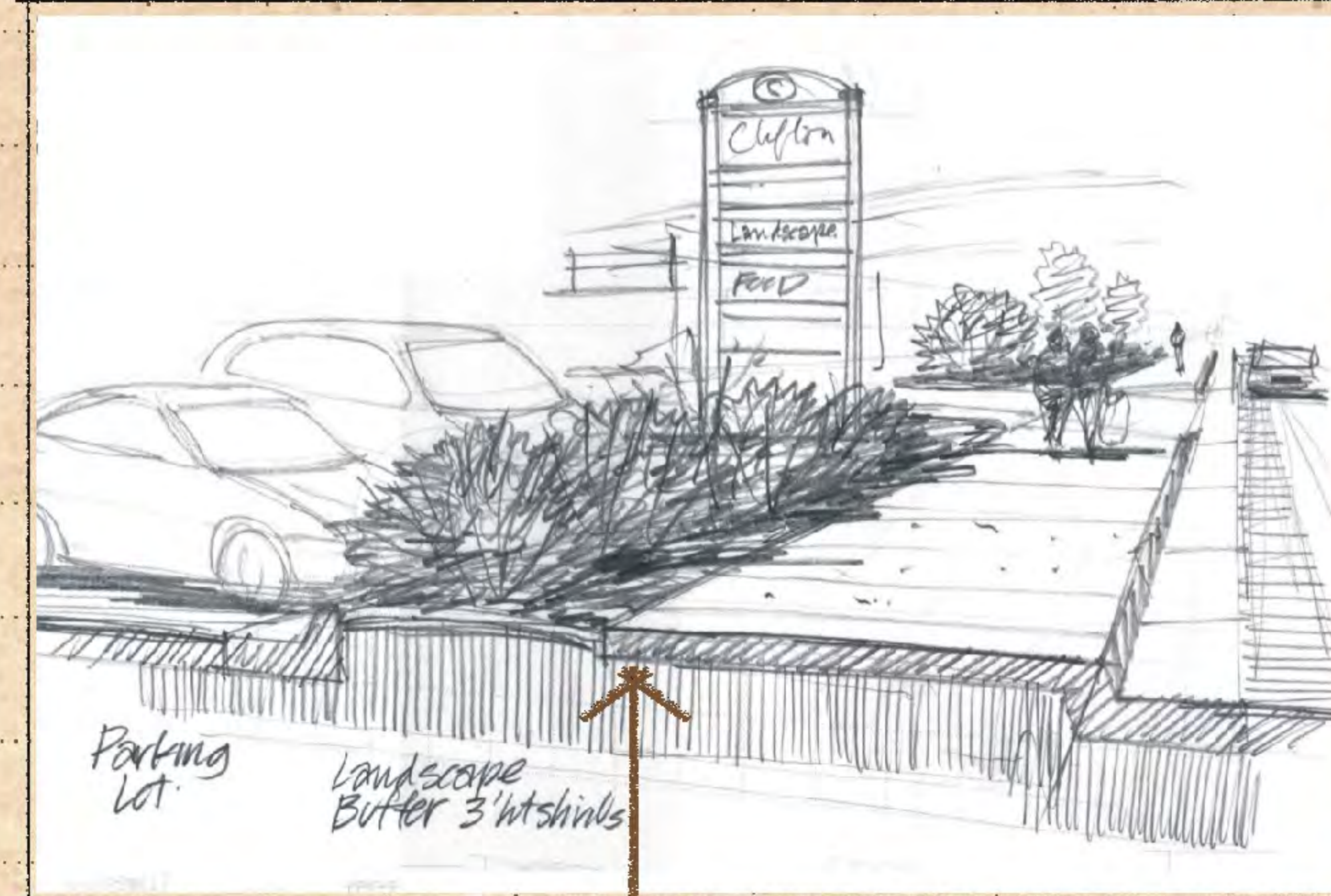
Sketch of low masonry wall. 3-4 ft ht. This can be either block or brick or combination of those. This can also be effective in commercial areas to block views of parking lots or in residential areas where more distant views are desirable.



Photo of solid wood fence with masonry columns. Although the masonry columns give it some refinement it has most of the disadvantages of a plain wood fence with minimal sound attenuation and a short life span.



Tall masonry noise barrier wall along a limited access highway, consisting of concrete block with bands of different colors. If it is absolutely necessary to have a tall wall for sound attenuation then this is one of the more effective examples. Pilasters and banded colors add interest. Coarse textured surfaces help reduce noise reflection. Some shrub planting along the face of this wall would soften its appearance as well as intercepting dust. Note there is no sidewalk or landscape strip on this roadside which gives the otherwise attractive wall a somewhat harsh look.



ABOVE: landscape zone between parking lot and sidewalk. This landscape buffer does a great deal to mitigate the harsh affects of pavements and parking lots in commercial areas. Tree planting needs to be strategically located so signs are not blocked. This landscape zone should be a minimum of 5 - 6 ft wide. Shrub species should not exceed 4 ft in height. Trees need to have a higher canopy to permit views under branches, to buildings.

WALLS, LANDSCAPE EDGES

Along arterial roadways

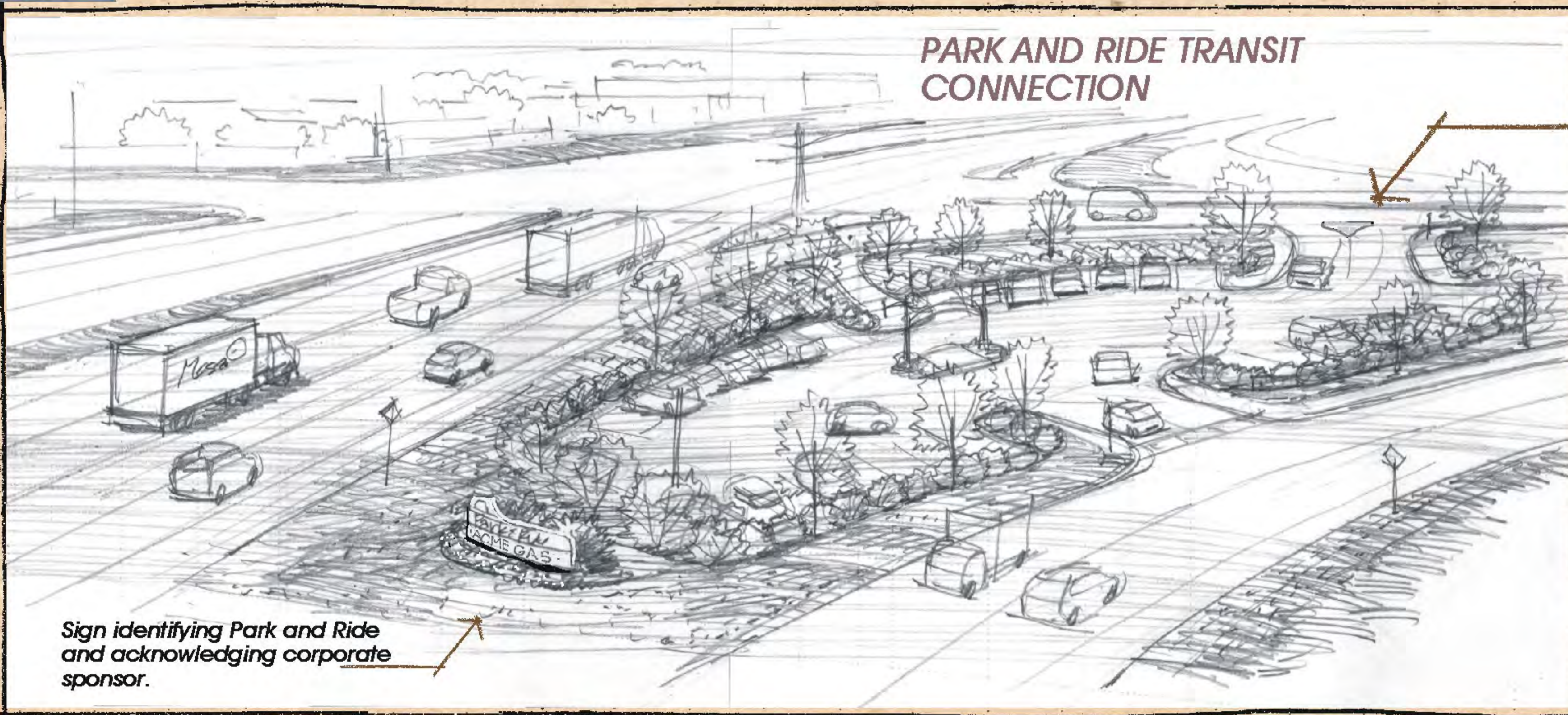
Masonry walls are the best sound attenuators. For residential area fronting on noisy busy arterial roadways brick or concrete block masonry walls are the most effective and arguably the most attractive solution. The tall 8-10 ft walls shown on photo at lower left are effective noise barriers. Careful treatment of masonry patterns and colors however, are important design considerations. A big blank wall with no color or refinement can be real negative element as well as an invitation to vandals. Even a low wall shown in the sketch (above left) helps reduce tire noise while not creating a 'canyon' feeling. The landscape strip mentioned previously is also important for mitigating the harsh effects of a tall wall, reducing dust, and deterring vandalism.

Other edges common to commercial strip areas are parking lot walkway edges. Views of lots of cars, and asphalt pavement can be obtrusive and harsh. A landscape zone with 3-4 ft ht dense shrubs and plants can do a lot to soften those negative views. Flower planting and tree planting are also important. Tree planting should be a little more widely spaced to allow views of stores and signs.

Edges, fences, noise barriers, parking lot edges

* *Masonry walls are the best noise barriers, but need to be designed carefully*

* *Parking lots should have some edge landscape by sidewalks*



PARK AND RIDE TRANSIT CONNECTION

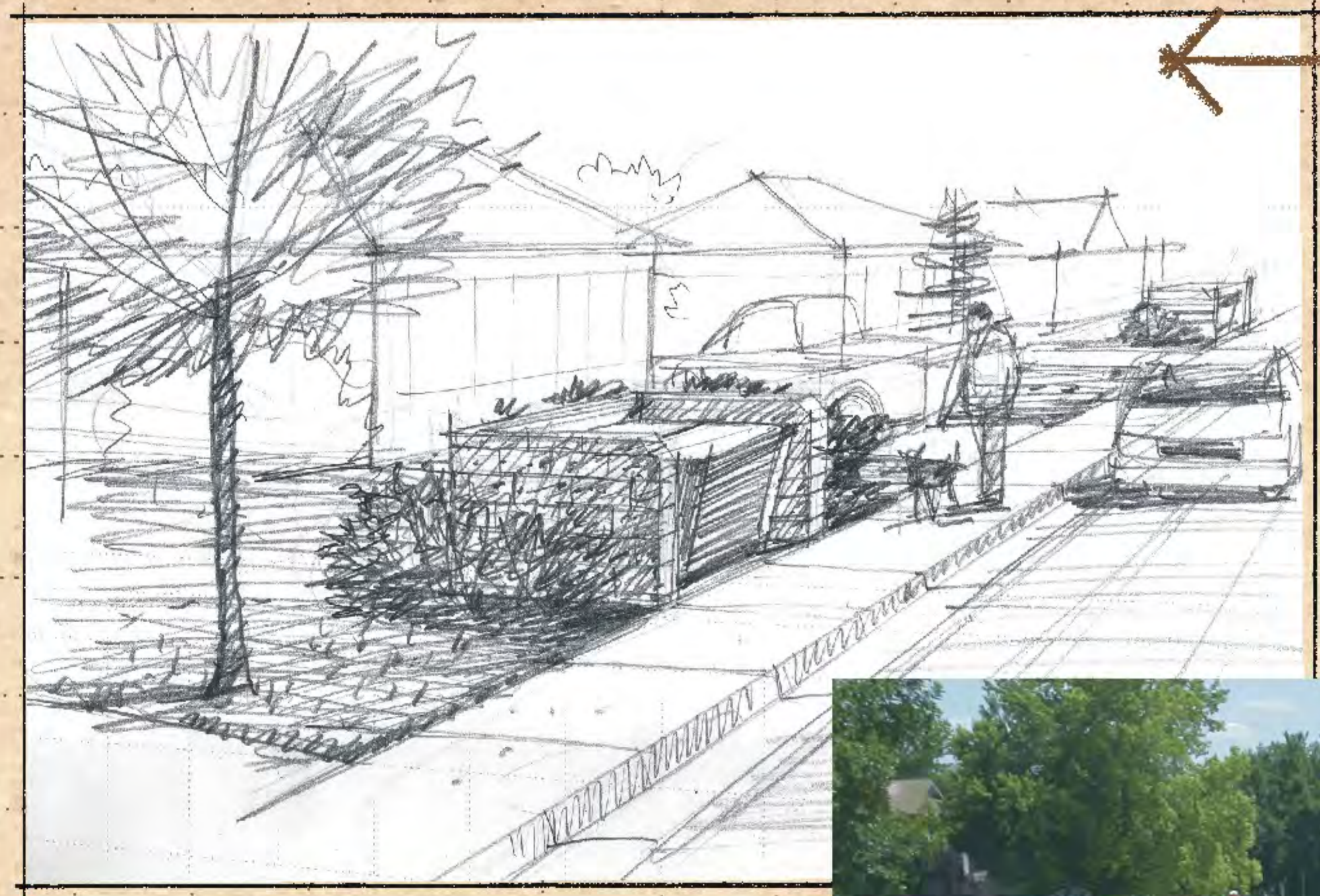
Two access points.
 Landscape of trees and shrubs around periphery of parking.
 Walkway connections,
 Adequate lighting, trash containers
 Local storm water detention may also be possible in these areas. Parking area grading can be designed to accommodate some storm water detention.

Sign identifying Park and Ride and acknowledging corporate sponsor.

PARK AND RIDES
Corporate sponsored
 A number of informal parking areas have developed along the I-70 Business Loop, along the adjacent frontage road. This the primary arterial entry way for the Clifton / Fruitvale area and these scattered parking areas are a negative visual distraction for such an important roadway. It is suggested here that these park and rides could be legitimized and improved with paving and landscape. Larger regional businesses may sponsor the construction and maintenance and get a sign acknowledging their community contribution.

* Corporations, maintenance districts, or citizen's groups can be important sponsors of community enhancements

Park and Rides, dumpsters



LOWER LEFT; Some dumpsters have to be directly on the street for access. A small block masonry wall combined with some shrub planting can reduce the obtrusiveness



RIGHT: A heavy wood fence and landscape are used to soften the negative appearance of a parking lot dumpster.

DUMPSTERS
They don't have to be that ugly
 Dumpsters for trash collection are a necessary evil. They need to be in an accessible and often a conspicuous location. A series of beat up containers lining the street can be an eyesore. There are ways though to create a more discrete enclosure with masonry walls or heavy wood fence as shown here.

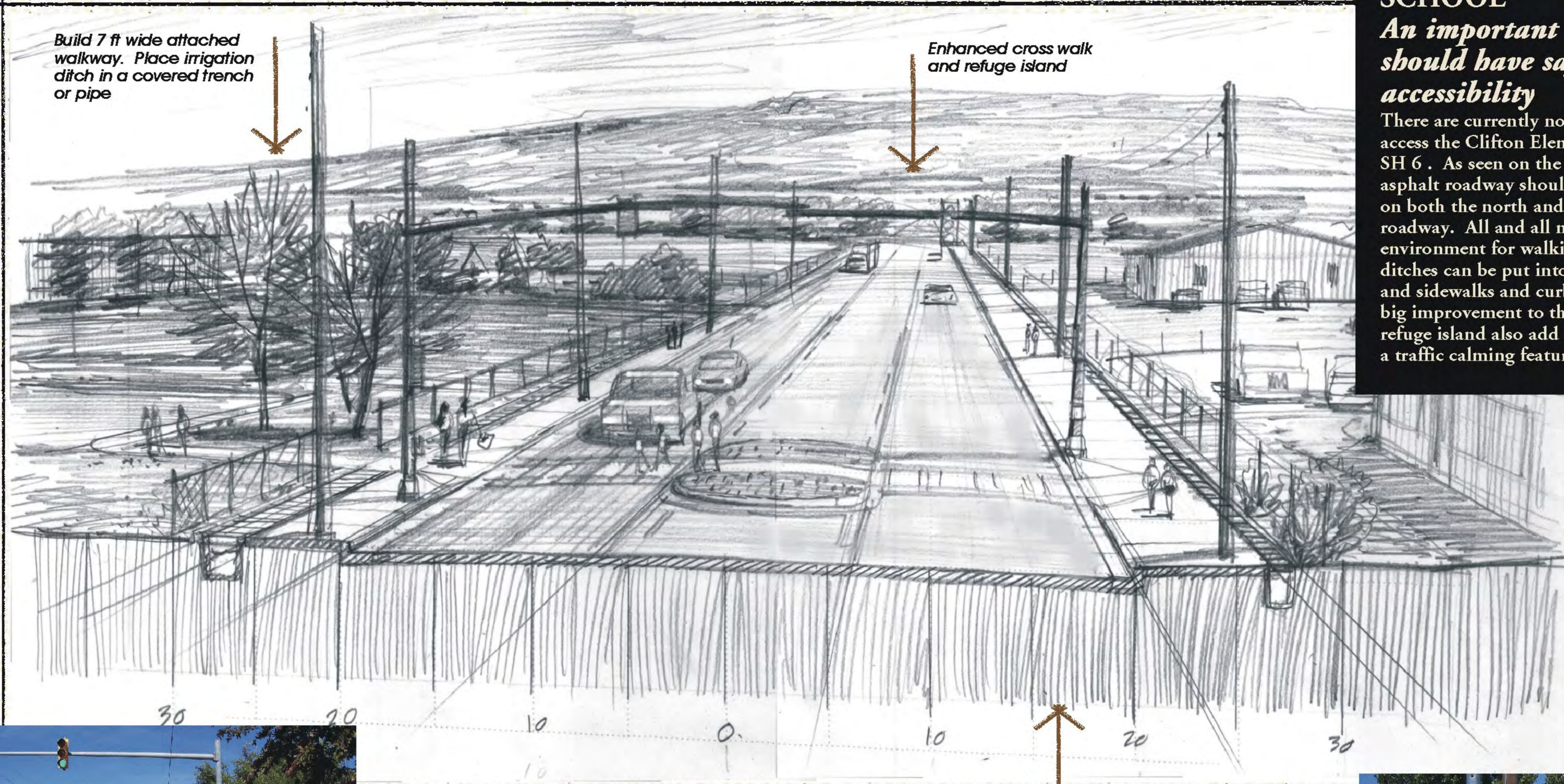
* There is hope for even the ugliest of urban features.



CLIFTON ELEMENTARY SCHOOL

An important community node should have safe pedestrian accessibility

There are currently no continuous walkways that access the Clifton Elementary school from F Road, SH 6. As seen on the photos there are 4-6 ft wide asphalt roadway shoulders and an irrigation ditch on both the north and south sides of this busy roadway. All and all not a safe pedestrian environment for walking to school. If irrigation ditches can be put into trenches covered with grates and sidewalks and curbs added it would make an a big improvement to the pedestrian safety. A median refuge island also add some safety as well as acting as a traffic calming feature.



Clifton Elementary School



LEFT: Looking west along north side of F Road note wide paved shoulder and irrigation ditch

View to the east at the school cross walk area of F Road, one through lane each direction with center left turn lane.

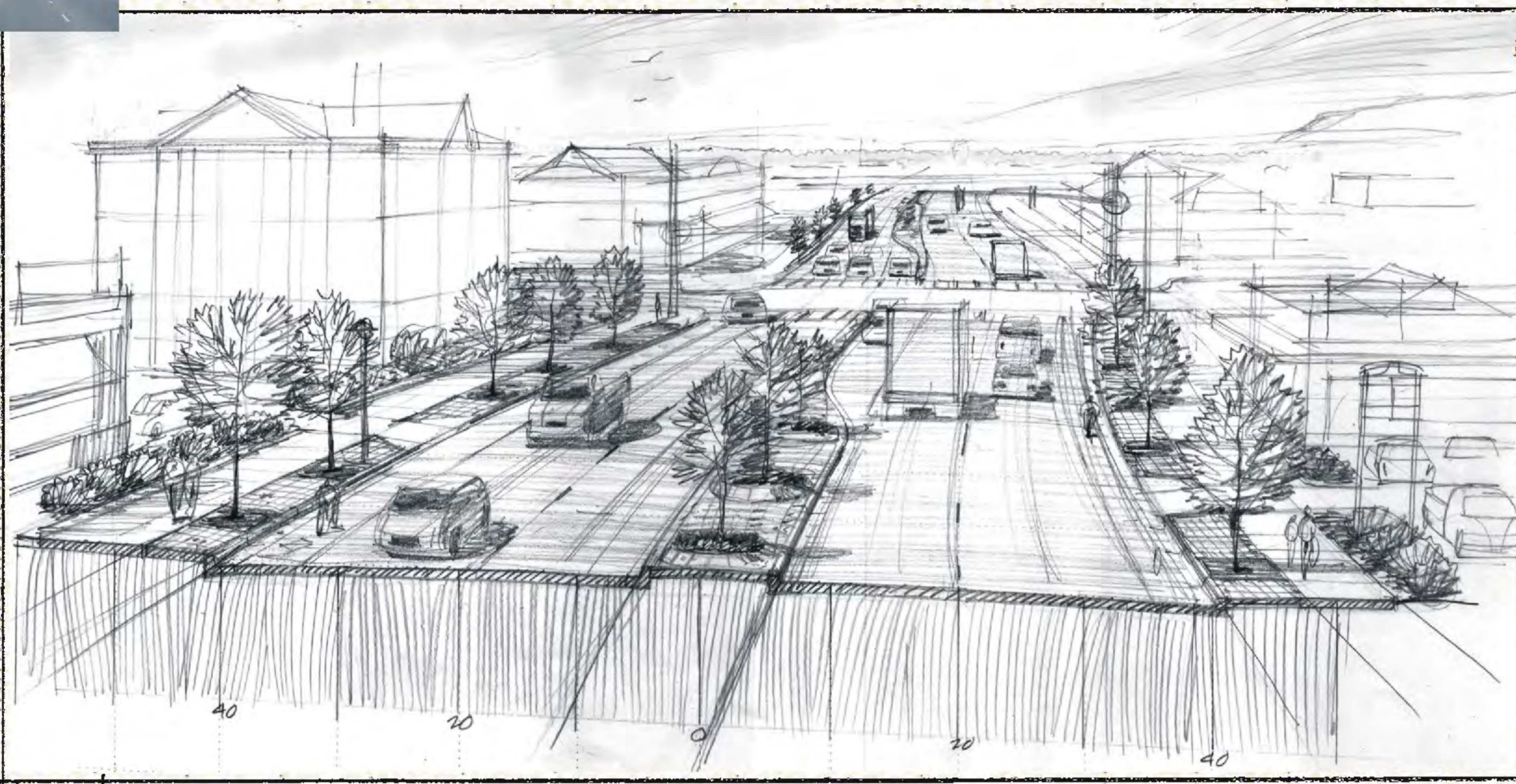
Safe pedestrian crossing with walk signals and enhanced cross walks



Looking west, south side of F Road near the Clifton Elementary School.

** One idea for improving pedestrian access to this important community node.*

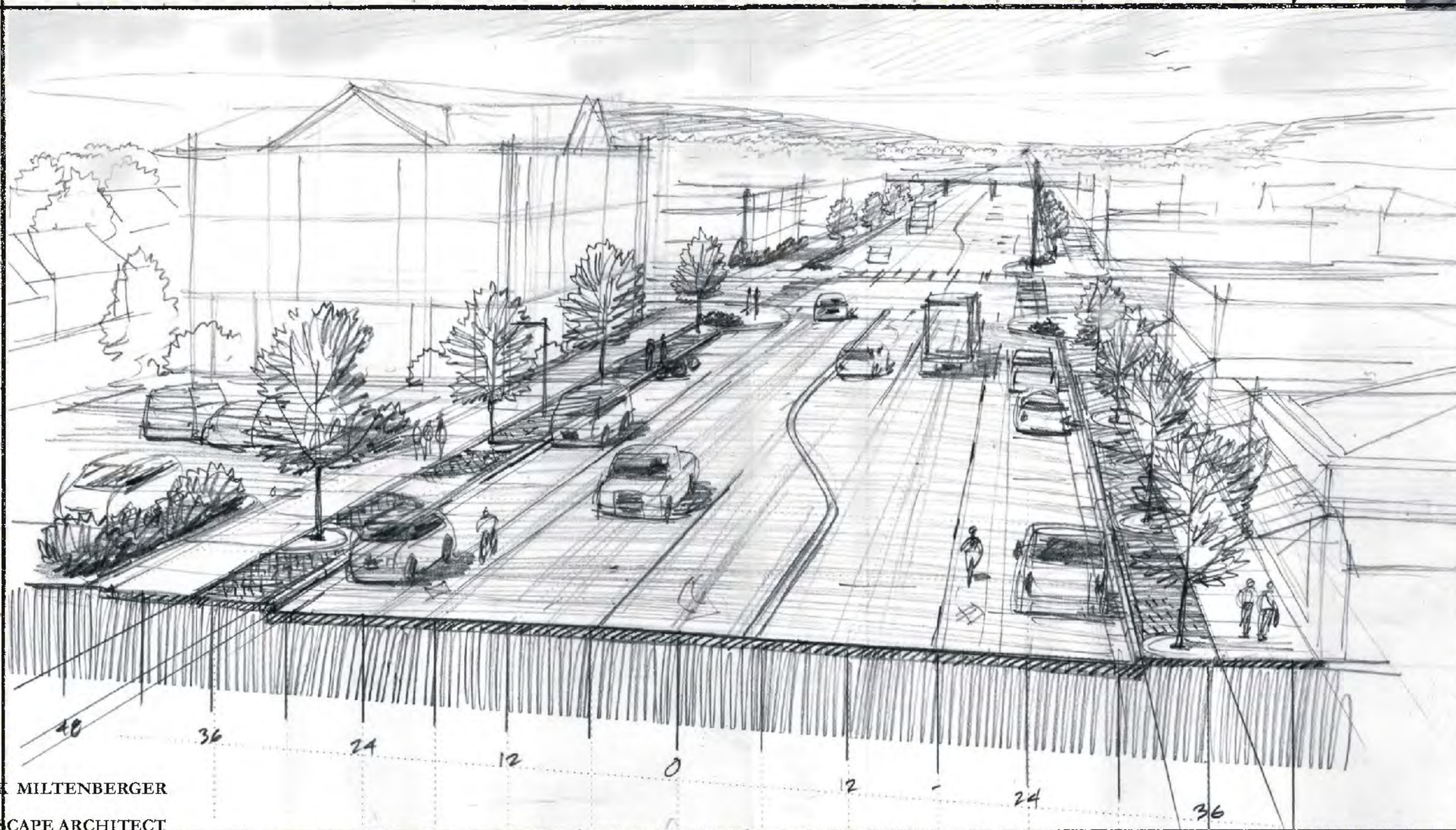




LEFT: Four lane alternative. This one has a landscaped median and a slight curve in the alignment to add interest and avoid important buildings. Bike lanes are also included. Parking for businesses would be to the sides and backs of buildings that front close to the street. Once again pedestrian friendly wide sidewalks and tree planting will encourage local pedestrian activity. This alternative would require more land purchase and would be more expensive than the narrower concept below.



ABOVE: Project area



LEFT: two lane alternative, with center left turn lane and on-street parking. This alignment will require only minimal right-of-way land purchase. Wide sidewalks and tree planting are also part of this scheme. Note the 'chokers', or constrictions, at the intersections. These are intended to enhance the important pedestrian crossings, and act as traffic calming features.

'F' ROAD DOWNTOWN CLIFTON

Between I-70 Loop and Clifton Elementary School

High traffic volumes, intermittent sidewalks, and related problems give this important state and local road an unfriendly feeling. Getting to the post office can be difficult. Planning for future improvements will consider a number of options, or combinations of options. These two sketch concepts include some ideas for consideration. Both would enhance the pedestrian accessibility. Handling the most traffic volume in the safest way is the highest priority.

- * Enhancements to the streetscape make for a vibrant commercial core at a human scale.
- * Simple aesthetic treatments can help retain and attract commerce/businesses
- * Creating attractive urban spaces in commercial areas can create opportunities for community events and gatherings. Farmers markets, sidewalk sales, musical entertainment, make neighborhoods fun places to live and visit.

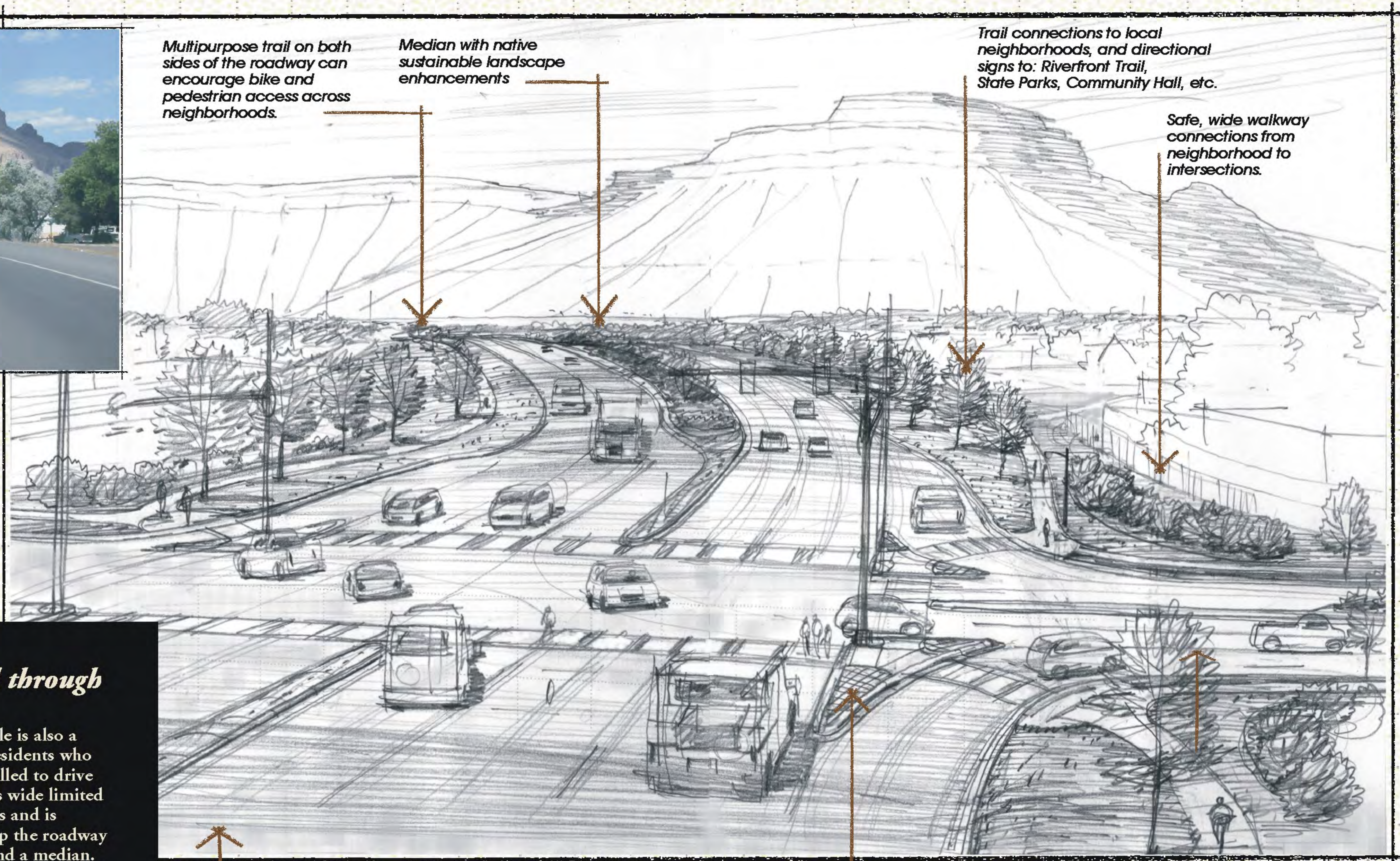
Old downtown should be a safer more friendly place to visit or to do business.

F Road, downtown Clifton



ABOVE : Existing northbound view of I-70 Business loop. Note the wide shoulders and median. Note also the commanding view of Mt. Garfield and the Book Cliffs. From this view point existing trees create an attractive middle ground.

Enhanced landscape of tree planting along roadsides and intersections.



I-70 Business Loop

I-70 BUSINESS LOOP
The primary arterial spine road through Clifton / Fruitvale
 This primary entrance arterial into Clifton Fruitvale is also a major impediment to community connectivity. Residents who live less than a quarter mile from stores feel compelled to drive because of the harsh pedestrian environment. This wide limited access highway has few pedestrian accommodations and is intimidating at best. This concept would tighten up the roadway cross section to a more urban arterial with curbs and a median. The addition of enhanced cross walks and traffic islands will help with pedestrian accessibility. A detached multi purpose trail is also suggested for both sides of the roadway as well as roadside landscape upgrades.

Bird's eye view looking north, sketch concept for enhancing this arterial roadway.

Safe pedestrian crossing with walk signals and enhanced cross walks

** This busy somewhat scary arterial roadway has the potential to become a more friendly attractive gateway and community connector.*



PART II SPECIFIC AREAS - I-70 Clifton Exit, Gateway enhancement



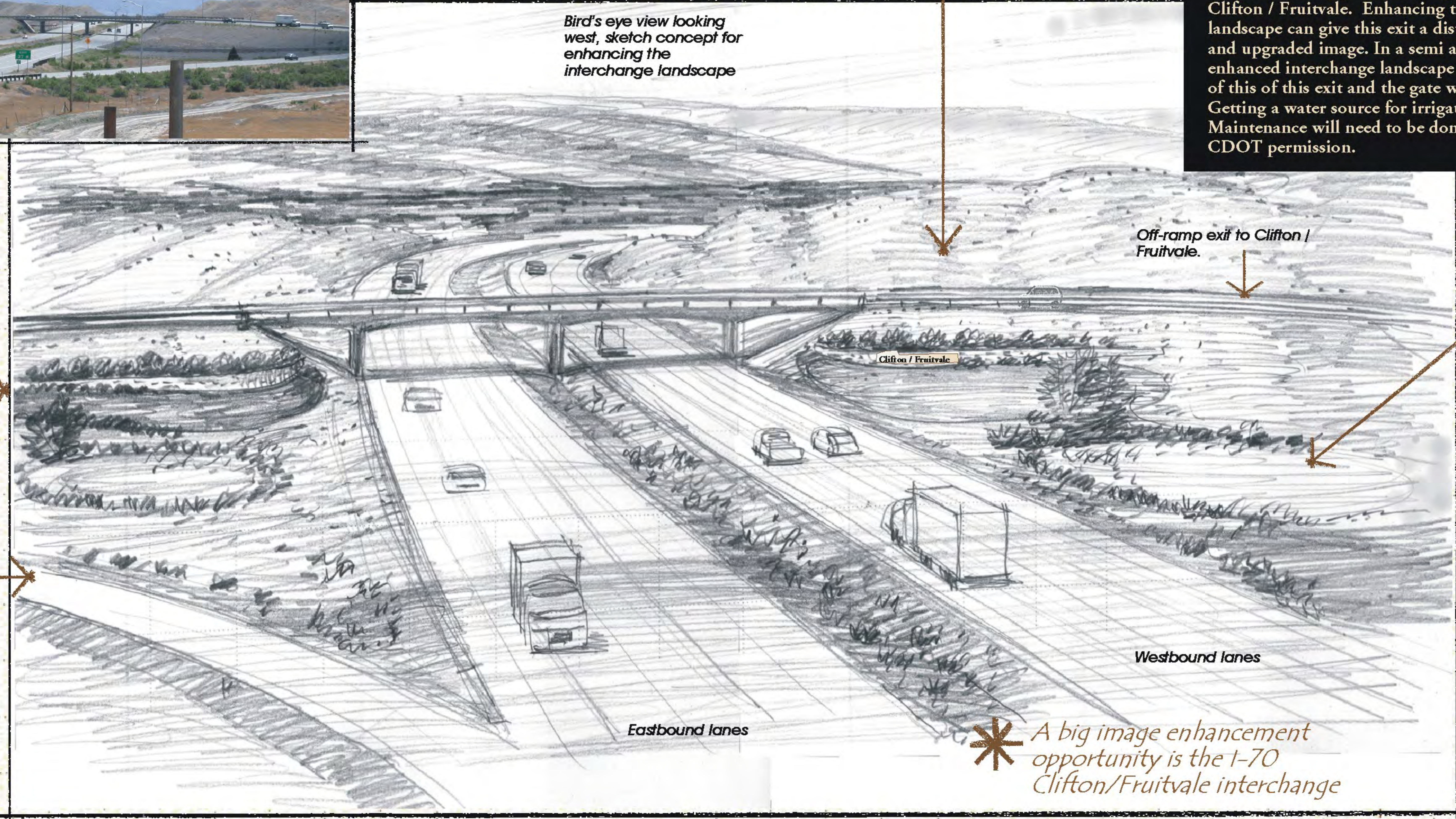
I-70 CLIFTON EXIT GATEWAY
Enhancing the interchange landscape can provide important visibility for Clifton /Fruitvale
 Interstate 70 is the primary connection to the state for the traveling public and regional commerce for Clifton / Fruitvale. Enhancing the interchange landscape can give this exit a distinguishing image and upgraded image. In a semi arid desert a greener enhanced interchange landscape can raise the profile of this of this exit and the gate way it represents. Getting a water source for irrigation may be an issue. Maintenance will need to be done privately with CDOT permission.



View to the west toward Clifton Interchange. Barren dry slopes give this interchange a stark dusty look. Conspicuous embankment slopes provide good enhancement opportunity.

Enhanced landscape at conspicuous bridge embankment slope area. May include stone masonry wall, tree and shrub planting, a small patch of irrigated grass and a sign plaque

Bird's eye view looking west, sketch concept for enhancing the interchange landscape



South side of I-70 conspicuous embankment slope can have similar treatment to north side with a rock wall, tree and shrub planting and some irrigated turf.

Off-ramp exit to Clifton / Fruitvale.

Possible storm water detention and water quality planting

Eastbound on ramp

Eastbound lanes

Westbound lanes

***** A big image enhancement opportunity is the I-70 Clifton/Fruitvale interchange

I-70 Clifton Exit Gateway enhancement



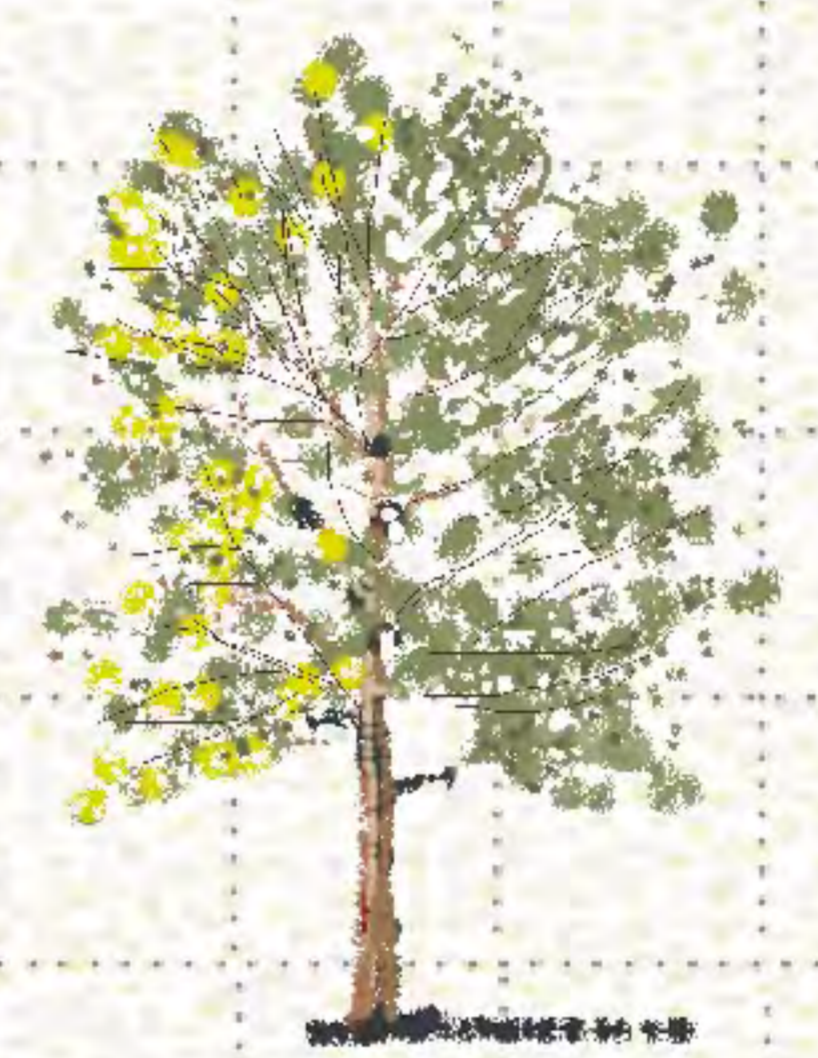
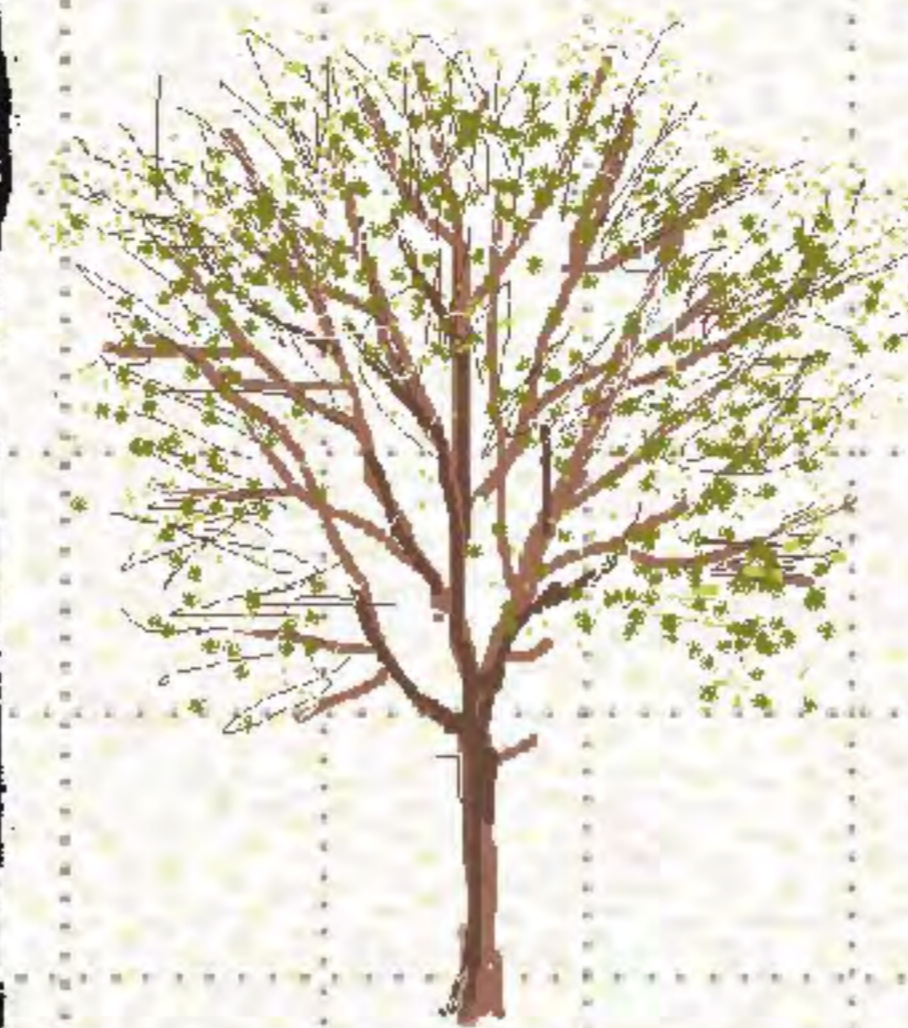
LOCAL RESIDENTIAL STREETS-
all of the roadside landscape is privately installed and maintained. Home owners can be offered incentives such as discounted 'arbor day trees' and given advice on species, spacing and planting techniques. Sidewalks should be required for all private development



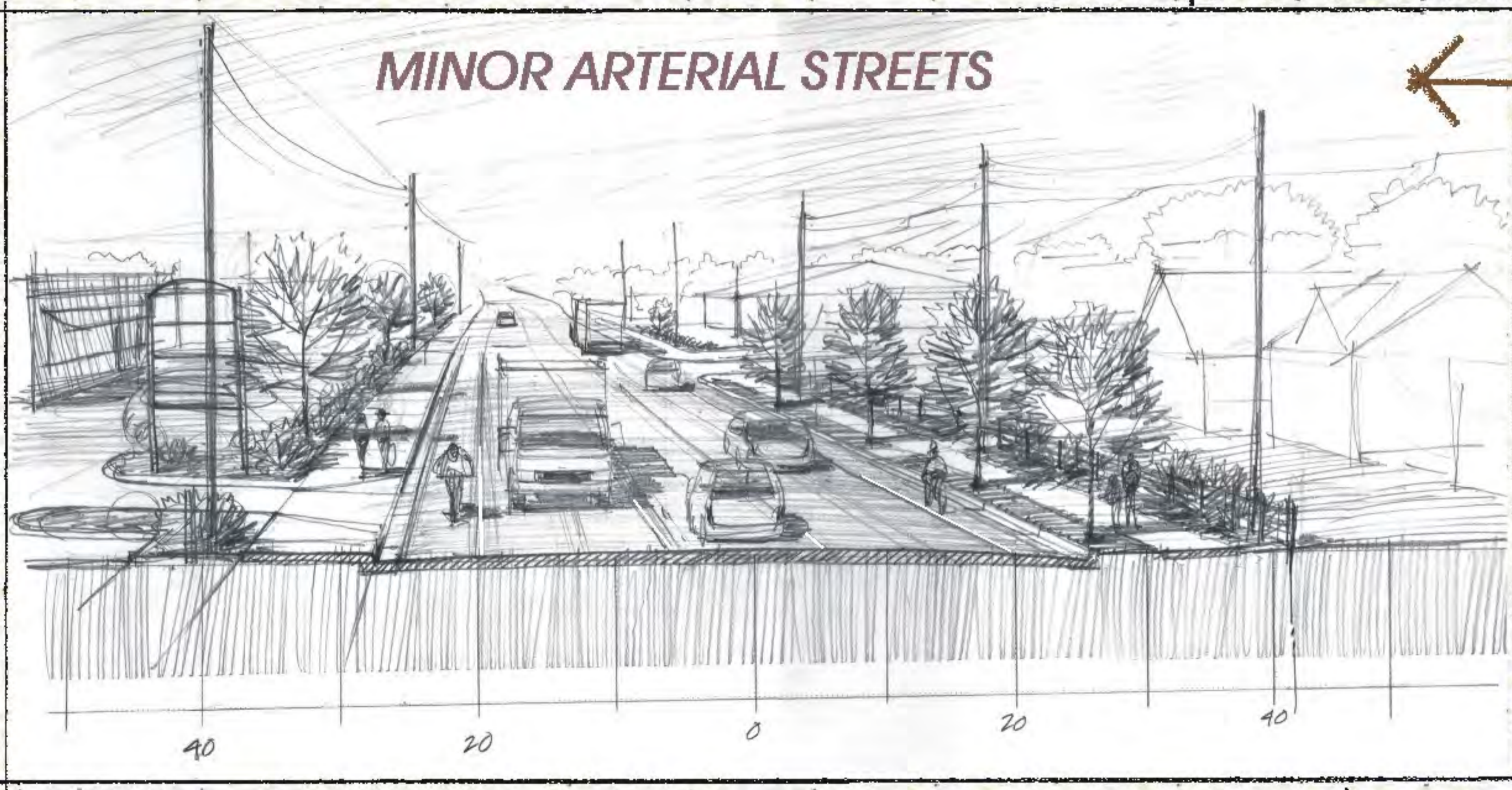
LOCAL RESIDENTIAL STREET

LANDSCAPE MAINTENANCE STRATEGIES
Combination of private and public maintenance is recommended.

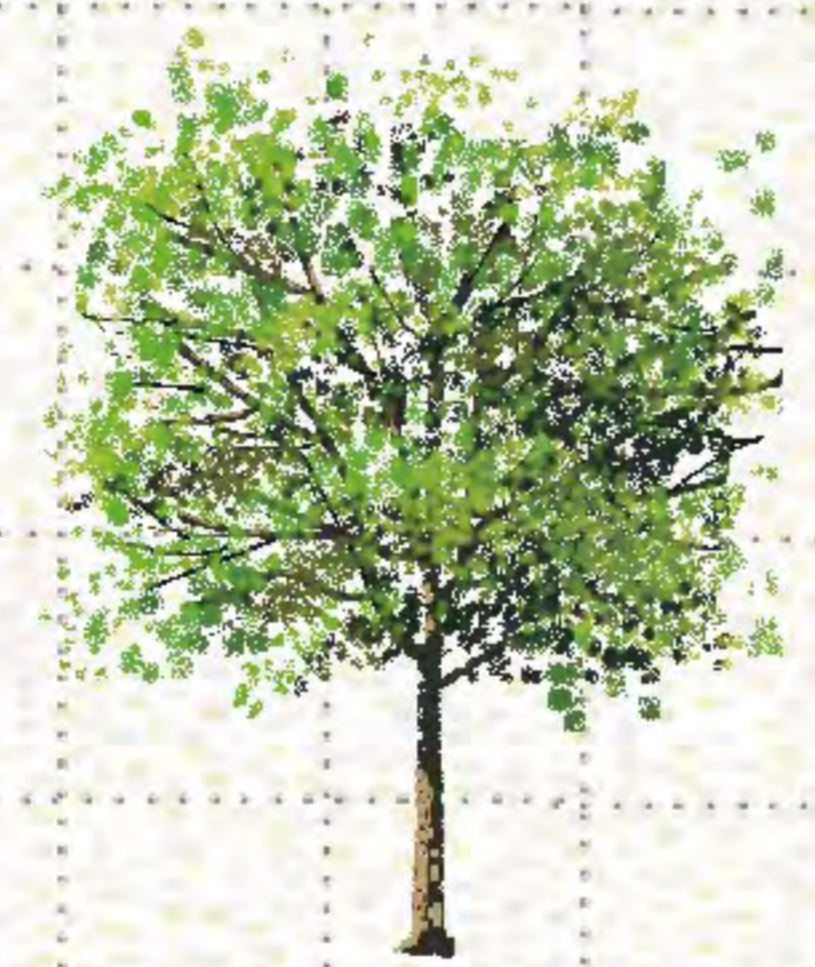
It is in the public's best interests to have pleasant attractive walkable public streets. The residents feel better about their neighborhood and property values are enhanced. Tree planting programs or incentives can encourage public rights of way landscape enhancements. Planning regulations can require these improvements. Maintenance districts can be created for maintaining roadway medians or unique public landscapes.



MINOR ARTERIAL STREETS

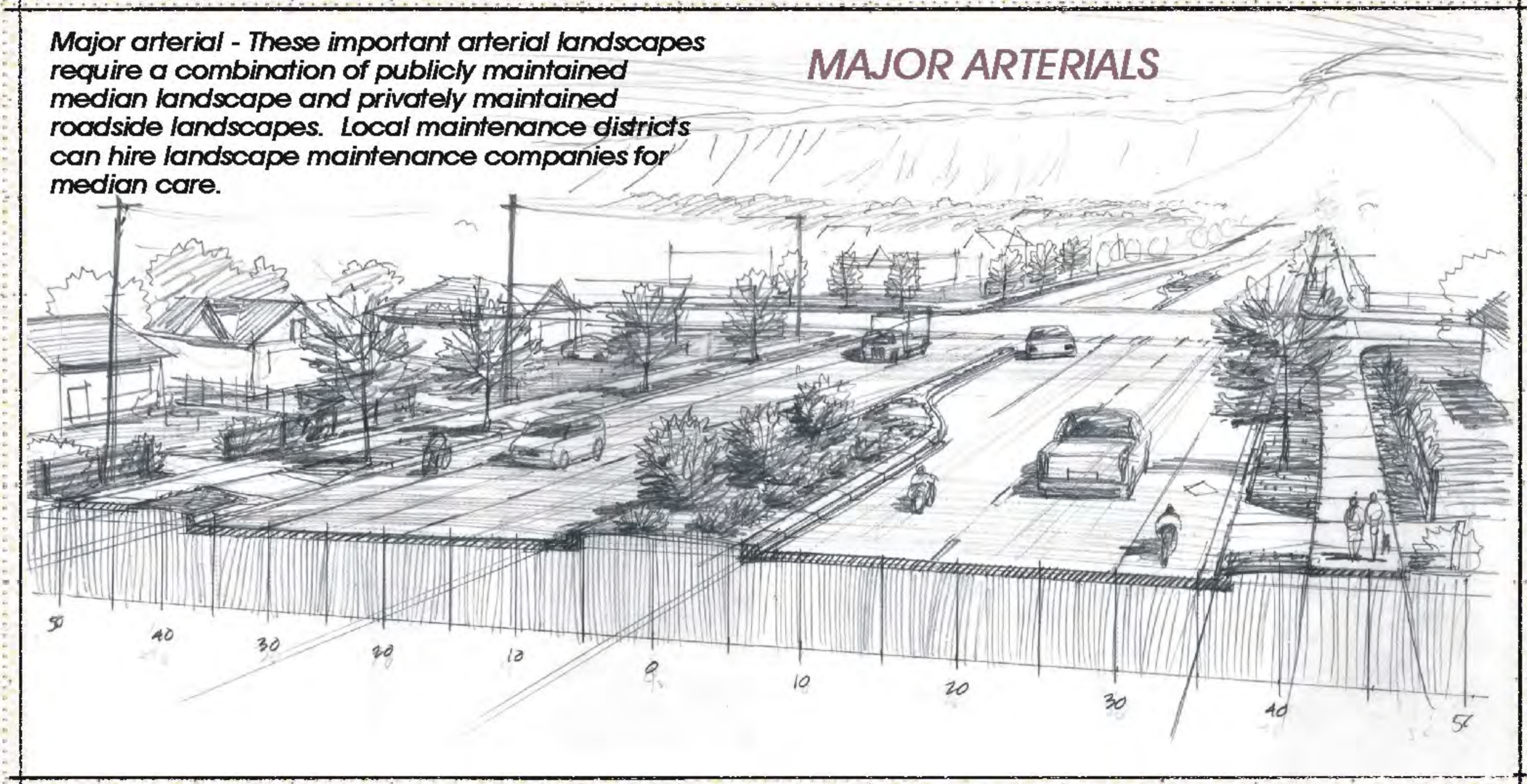


LEFT; Minor arterial streets, encourage property owners to plant trees by providing discounted trees and planting information. For edge planting, encourage building department to make a landscape zone a requirement for edges and fence construction. Sidewalks should be required as a part of property improvements.

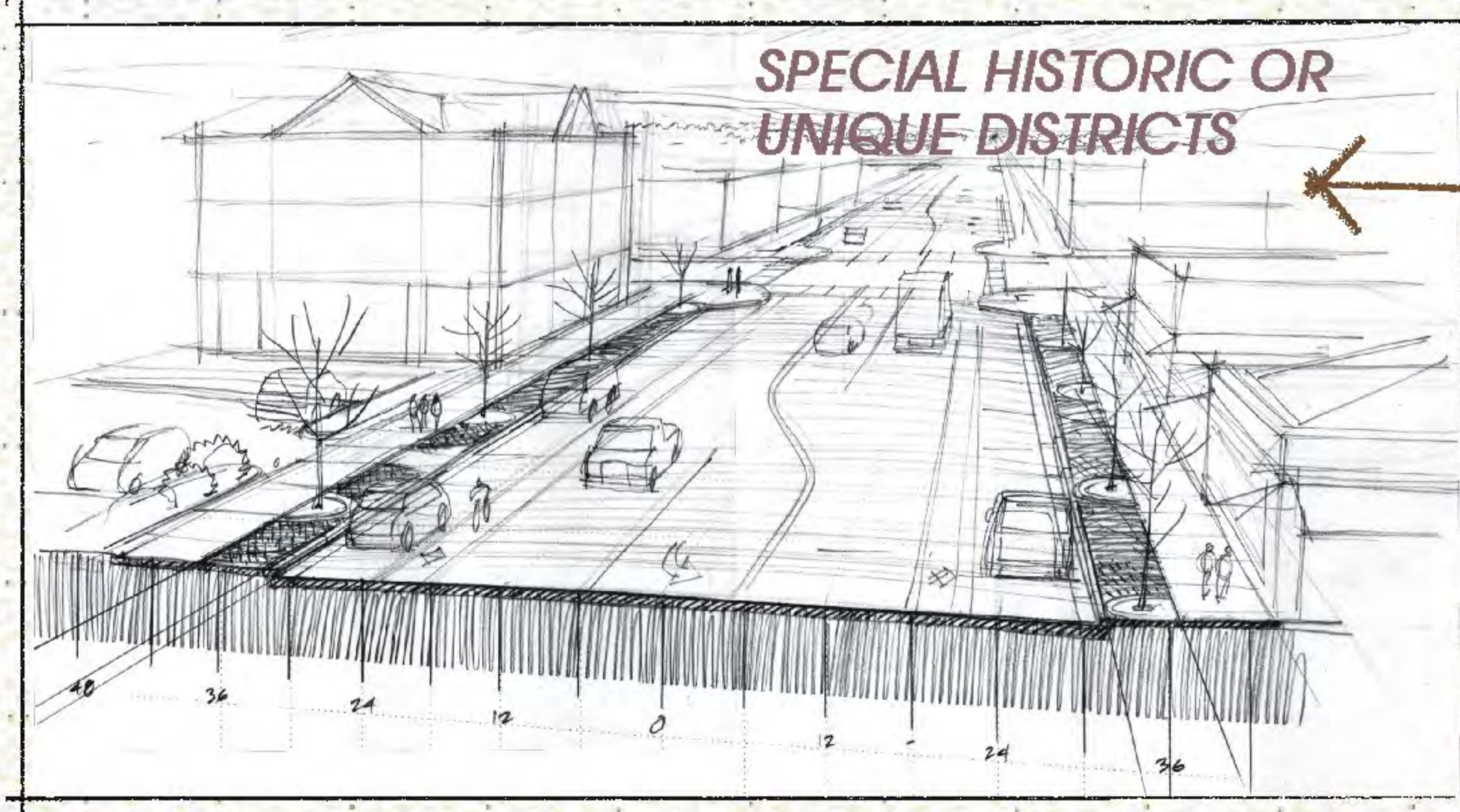


Major arterial - These important arterial landscapes require a combination of publicly maintained median landscape and privately maintained roadside landscapes. Local maintenance districts can hire landscape maintenance companies for median care.

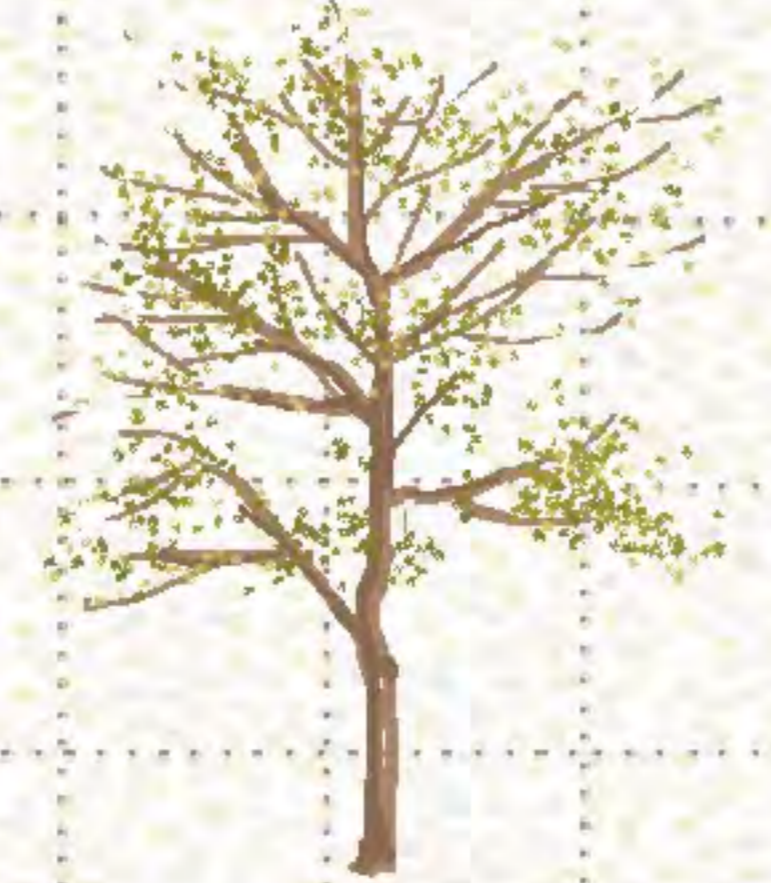
MAJOR ARTERIALS



SPECIAL HISTORIC OR UNIQUE DISTRICTS



LEFT: Historic districts. Streetscape grants can be made available for designing and funding construction. Special maintenance districts can be created to maintain the roadside landscapes, or landscapes can be maintained under written agreement.



* Public / private cooperation can create livable and attractive communities.